



# Wayfinding design guide

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Our public spaces play a fundamental role in ensuring the Royal Docks is a sustainable, healthy and inclusive place, and one that meets the needs of the community and creates a unique place in London.

The Royal Docks Public Realm Framework, published in March 2020, sets out the vision and strategy to ensure the delivery of the Royal Docks' public realm is comprehensive, coherent and connected, while also ensuring a diverse approach. Achieving this vision relies on working closely with a number of stakeholders and partners, so it is important that the principles of the Framework are translated into clear guidance for delivery.

This is why the Royal Docks Team has developed a series of Design Guides, by working with local stakeholders and communities to adopt a user-centred approach. These Guides set out how the design principles of the Public Realm Framework should be applied across the Royal Docks.

The Design Guides support the Mayor of London's Good Growth by Design agenda and the London Borough of Newham's Community Wealth Building agenda. The Guides contribute towards the creation of thriving "15-minute neighbourhoods" that connect communities to critical services and social and civic amenities, supporting the health, wellbeing and prosperity of everyone who lives and works here. The Guides focus on equalising access to public space and prioritising sustainable travel modes through inclusive design principles; and they help fulfil air quality and climate change adaptation commitments.

Three thematic Guides have been created: the Wayfinding, Lighting and Landscaping Guides enhance the character and legibility of the place. They focus on several fundamental and cross-cutting principles, including;

- Creating places that are inclusive and accessible.
- Increasing access to green spaces and local amenities.
- Improving connectivity across the Royal Docks.
- Enhancing access to the water.
- Celebrating the heritage and culture of the Royal Docks.
- Creating a welcoming and safe environment.
- Promoting healthier lifestyles.
- Promoting community participation and co-design.
- Encouraging community stewardship of public spaces.

An Inclusive Design Guide will accompany and support the other three thematic Guides, setting out how national legislation and best practice should be applied to the specific accessibility challenges characterising the Royal Docks' open spaces, so that we can ensure equitable access for all.

The Design Guides constitute an essential resource for anyone who intends to commission design work and public realm projects in the Royal Docks, including public sector organisations, developers, landowners, local businesses and community organisations.

The Guides are intended to sit alongside and support the vision and principles of other strategies, including the Royal Docks Economic Purpose and the Royal Docks Cultural Placemaking Strategy, which are key to shaping the regeneration of the Royal Docks. Taken together, the principles will ensure the Royal Docks becomes a testbed for new ideas and innovation, and a unique place that benefits communities, businesses and visitors alike.

# 1. Introduction



# The design guides

In 2019 the Royal Docks Team worked with 5th Studio to produce the Royal Docks Public Realm Framework (the Framework), which was endorsed by the Enterprise Zone Programme Board in March 2020. The Framework sets out the key principles for future interventions and investment in the area’s open spaces.

The Design Guides are the next phase of this project, establishing a unified design code for wayfinding, lighting and landscape elements in the area.

These guides are intended to support the delivery of a coherent, clear and socially inclusive public realm across the Royal Docks. The ambition for the transformation of the Royal Docks is set out in the Royal Docks Public Realm Framework, which was publicly consulted on in 2019 and published in March 2020.

The following pages summarise the key ambitions for the public realm and should be read in conjunction with the more detailed findings set out in the Framework. The summary of engagement, and a ‘How to use this Guide’ section, are also included here.

### Purpose of the Guides

The aim of the Design Guides is to provide a coordinated approach to landscape, lighting, wayfinding and inclusivity and access for the Royal Docks. As set out in the Framework, it is essential that interventions in the area are working towards similar goals and draw from a similar design language. The Design Guides will serve as a common resource to achieve this goal.

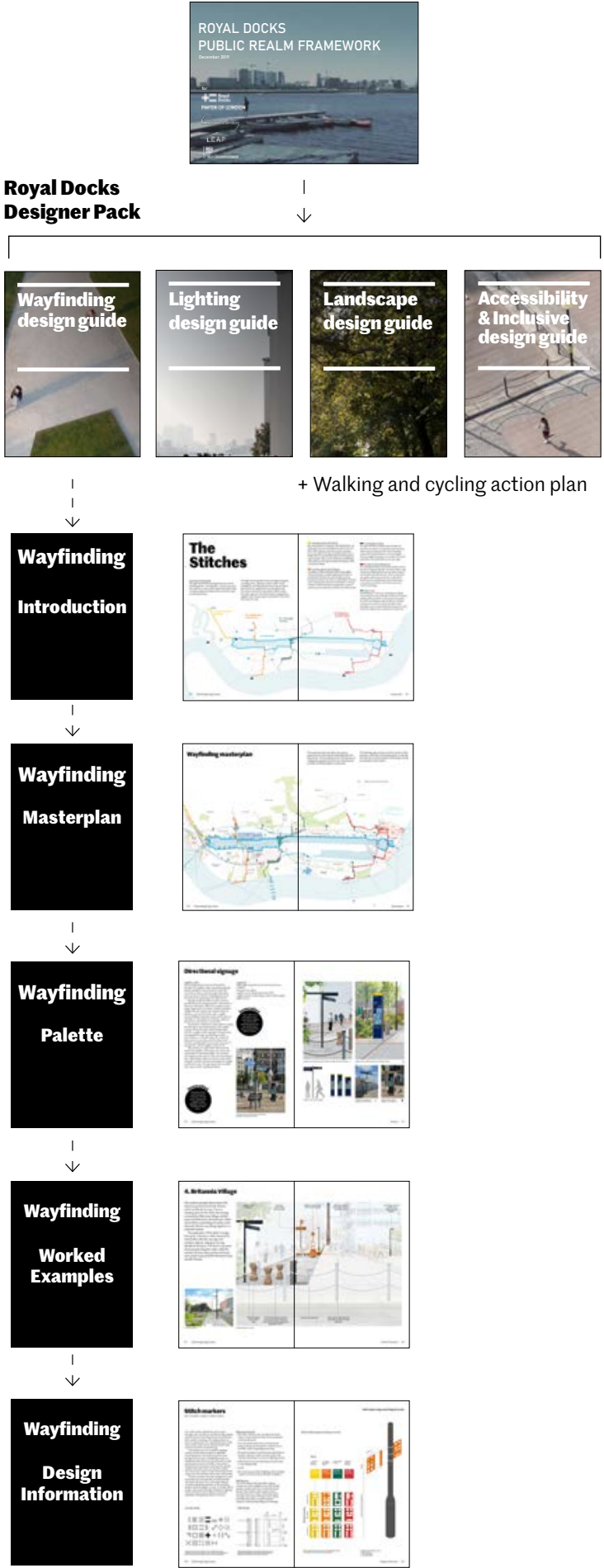
### Who are the Guides for?

The Design Guides are primarily for anyone who is thinking of commissioning or designing a project within the Royal Docks. As new development impacts everyone in the area, both existing and future communities, the Design Guides also provide a record of how community groups and key stakeholders have participated in and influenced the process.

### Structure of the Guides

The structure of the Design Guides will allow stakeholders, designers and local authorities to understand the vision for the area as a whole, as well as use the specific guidance within the Guides for different areas and conditions. An understanding of the broader picture is vital to all work conducted in the Royal Docks - even if the smaller projects are individual in nature, the value of these individual interventions can be diminished if the spirit of the overall area plan is not taken into account.

At the end of this chapter, a ‘How to use this Guide’ spread is included to ensure that the Design Guides are used effectively.



Public Realm Framework  
Sets out an overall vision for the Royal Docks. Identifies the challenges to overcome and the strategies to ensure this. Also identifies key areas where intervention is needed in order to achieve a coherent public realm.

Design Guides  
Identifies the specific strategies and elements that users of the Guide can employ to meet the objectives of the Framework. The Guides also add more detail on the elements that are appropriate to use in certain locations, as well as providing technical information and maintenance advice.

Sets out how the Design Guides build on the vision of the Framework, and also explains the process of engaging with stakeholders.

A series of diagrams and masterplan drawings, providing spatial representations of the proposed strategies and palettes for each discipline (landscape, lighting and wayfinding).

Describes in detail the palettes for each discipline, setting out when certain elements should be used and their desired impact.

Provides annotated views and diagrams showing how to combine elements from the palettes in example locations.

Provides further design information for selected elements.



# Stakeholder and Community Engagement Methodology

Royal Docks stakeholders and communities are the people who will use, apply and benefit from the Design Guides, as well as the resulting public realm. This is why their involvement has been critical to developing the Design Guides.

## Stakeholders

Landowners, developers, businesses, authorities and communities will use and apply the directions from these Guides in any public realm schemes they implement. Securing the input from these groups means the Guides respect key infrastructure, building or land requirements and restrictions, and this collaborative approach also secures long-term usability of the Guides.

## Communities

Local civil society groups, residents and workers are primary users of the public realm. Their input aimed to ensure the Guides were developed in response to people's everyday experiences and needs, as well as considering the needs of a diverse community.

The active participation of stakeholders and communities in the area's regeneration is central to the Royal Docks Delivery Plan and its Communities

Strategy. One of our key goals is continuous involvement, which strengthens the existing assets of the area and ensures that people feel invested in and committed to building on the community's resilience for the long term.

As such, this initiative was conceived as part of an on-going process to involve stakeholders and communities in the design and delivery of public realm across the Royal Docks. These Guides build on previous dialogue, with the goal of strengthening relationships and preparing the ground for future collaboration.

A preliminary phase collated and analysed information from previous engagement initiatives since 2018, including the initial 2019 Royal Docks public consultation that contributed to development of the Public Realm Framework. These findings informed the first drafts of the masterplans and fed into the design of how we should approach engagement.

The Design Guides were developed in stages, based on stakeholder and community input:

## Phase 1: Strategy, Principles and Masterplans

Drawing on input from stakeholders and communities, we shaped the strategic approach of the Design Guides, their overarching principles and the masterplans for lighting, landscape and wayfinding.

## Phase 2: Palettes

Input from Phase 1 and the resulting principles and masterplans were used to develop the approach to Phase 2, key locations and initial Palette proposals.

## Stakeholders

For Phase 1, we held large multi-stakeholder group discussions to collectively review existing design guidance and the Masterplan proposals, and this enabled us to identify key needs and issues. For Phase 2, we moved to smaller, more focussed sessions to gain recommendations for implementation, as well as drawing on particular perspectives or expertise for how to best manage specific locations and elements of Masterplans and Palettes.

## Communities

In order to draw out the wishes, requirements, and relevant expertise of the communities, we used an iterative process that incorporated feedback and learning loops to address gaps and meet changing needs. The process helped communities gain skills and the confidence required to lead their own activities. This created informed and safe spaces for user-centred feedback that drew from groups' own networks, encouraging participation and creative input from diverse sources.

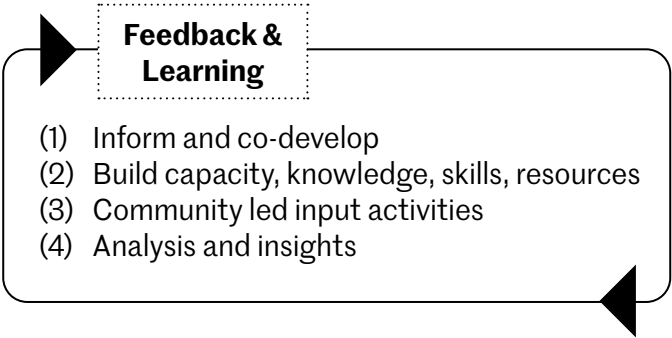
## Phase 1

Questions were designed to enable personal and social reaction, so we could capture lived experience and overall attitudes to public space. We also wanted to reflect the impact of Covid-19, as well as to gain specific location-based feedback on the Design Guide disciplines to inform the strategies and masterplans.

The questions were adapted for different engagement formats. We worked with community organisations to fit their programmes, including an online survey, befriending calls, a video, and online walking tours along the four Stitches in focus groups. Briefing sessions and facilitator packs, including maps and visuals, were developed to enable communities to lead their own activities.

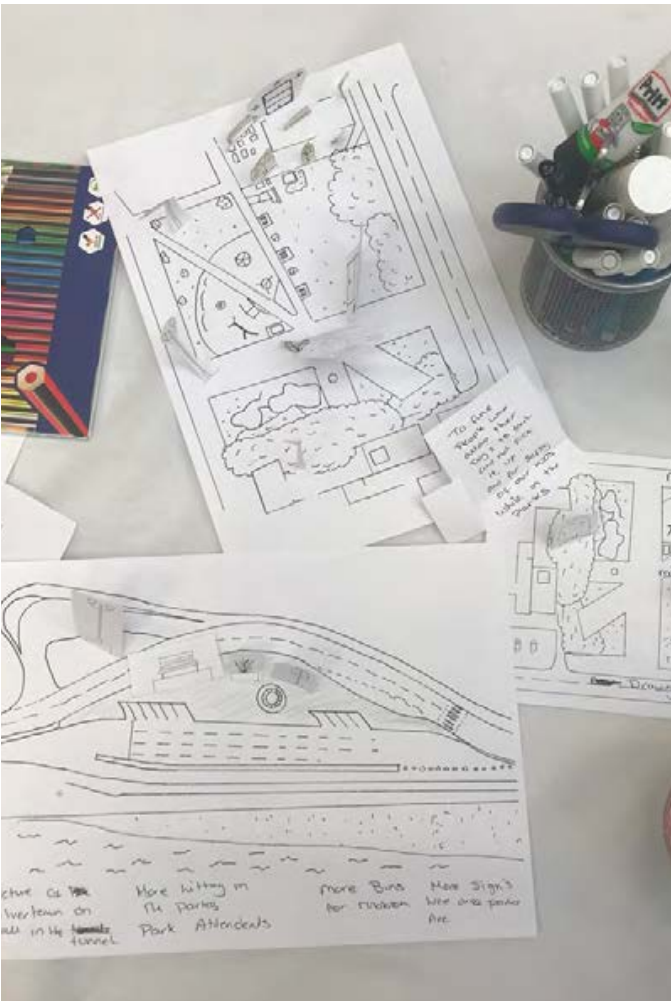
## Phase 2

Artist-facilitated creative activities were organised so we could glean specific design input, and use these to shape, adapt and add detail to the proposed palettes for each Design Guide. Sketches of Worked Examples enabled communities to imagine how elements of wayfinding, lighting and landscaping would come to together in a particular place, equipping them to make specific suggestions.



Left: The diagram shows how the involvement of stakeholder and communities followed different streams and used different approaches in order to meet the input and engagement objectives specific to each group.

Right: Creative activities resulted in a wide range of materials





# Stakeholder and Community Engagement Outcomes

Summary of stakeholder engagement outcomes  
The following points summarise the feedback from stakeholder workshops:

### Overall approach

- The proposed structure of the Guides is helpful to navigate and will be a useful resource to work across different landownerships.
- The Guides should establish overall principles, but be flexible and allow for local adaptation and interpretation in specific sites.
- Prioritise accessibility and inclusivity in the guides – they are key in promoting a diverse and connected environment in the Docks.
- Making the place more interesting and attractive is key to encourage more cycling and walking.
- Integrate heritage across the guides, from up-lighting to wayfinding and street furniture.

### Locations

- Sites for the Worked Examples were suggested Area-based sessions provided localised suggestions across the three thematic guides.

### Wayfinding

- Provide a more creative and integrated approach to wayfinding that is embedded in a wider narrative of the Docks as a place. While specific sites may have an internal signage approach, this should be clearly guided by common overarching principles.
- Prioritise navigation from public transport hubs and provide coherent signage between places and amenities. Stakeholders with large wall space are open to conversations about displaying public art and creative wayfinding.

### Landscape

- Provide a consistent approach to Dock edge protection, while encouraging the public to get close to the water.
- Set out guidelines for planting and appropriate species in light of local restrictions.
- Integrate elements to counter pollution such as environmental walls. Hard landscaping should provide smooth surfaces to support accessibility for less mobile people, buggies and active travel.

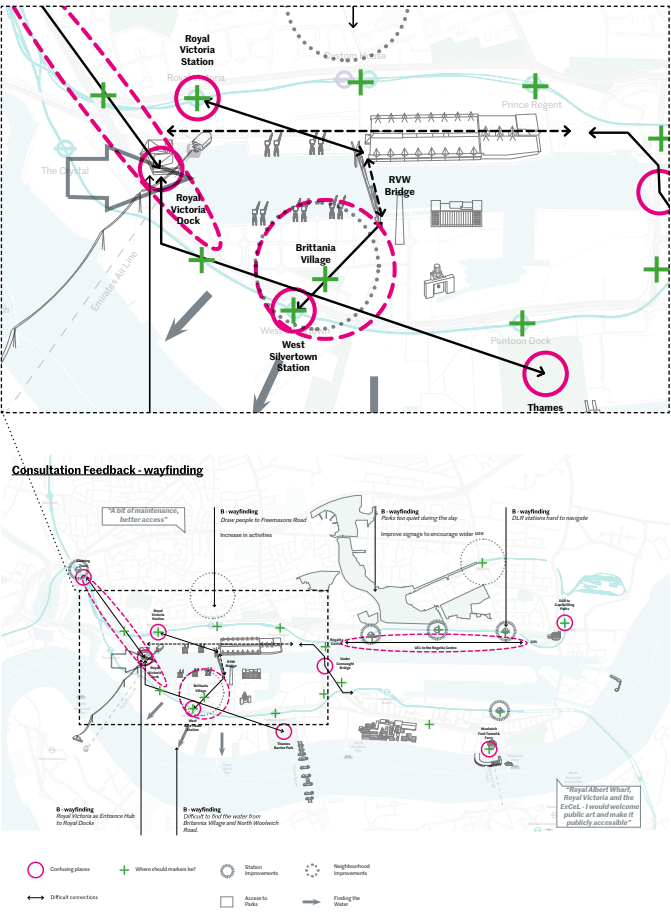
- Guidance around maintenance and use of resources should be an integral part of the Landscape Guide.
- Lighting**
- Create a consistent approach to lighting to reinforce the stitches and a hierarchy of routes.
  - Lighting design should incorporate flexibility to cater for different uses at different times while recognising conflict.

### Cross-cutting

- Acknowledge sustainability issues around technological implementation. Explore institutional partnerships for developing innovative solutions.
- Optimise access to the Water for multiple uses.

### Community engagement analysis framework

An analysis framework was developed to collect, process and analyse the responses. This incorporated thematic analysis (Braun & Clarke, 2006 ), and human centred design analysis such as user stories. The framework also incorporated spatial analysis in the form of Landscape, Lighting and Wayfinding Feedback Maps, which plotted key themes along the Stitches to inform their treatment.



Mapping feedback relating to wayfinding

Braun, V., & Clarke, V. (2006). Using thematic analysis in psychology. Qualitative Research in Psychology, 3, 77–101.

Summary of community engagement outcomes  
Survey responses, verbal feedback, annotated maps, drawings, photos, video created a rich feedback picture. Analysis of personal and hyper-local experience was brought into the conversation in the form of area-wide challenges and opportunities to inform multi-scalar strategies for Wayfinding, Landscaping and Lighting.

Community feedback highlighted hot-spots of multiple activity outside the Stitches, demonstrating how different strategies work together. This informed the Worked Examples for Phase 2: defining locations and combinations of palette elements to test the viability of approaches amongst stakeholders and communities.

Below are a few examples showing how community feedback analysis informed Strategies, Masterplans and Palettes at different scales and levels.

### Landscape

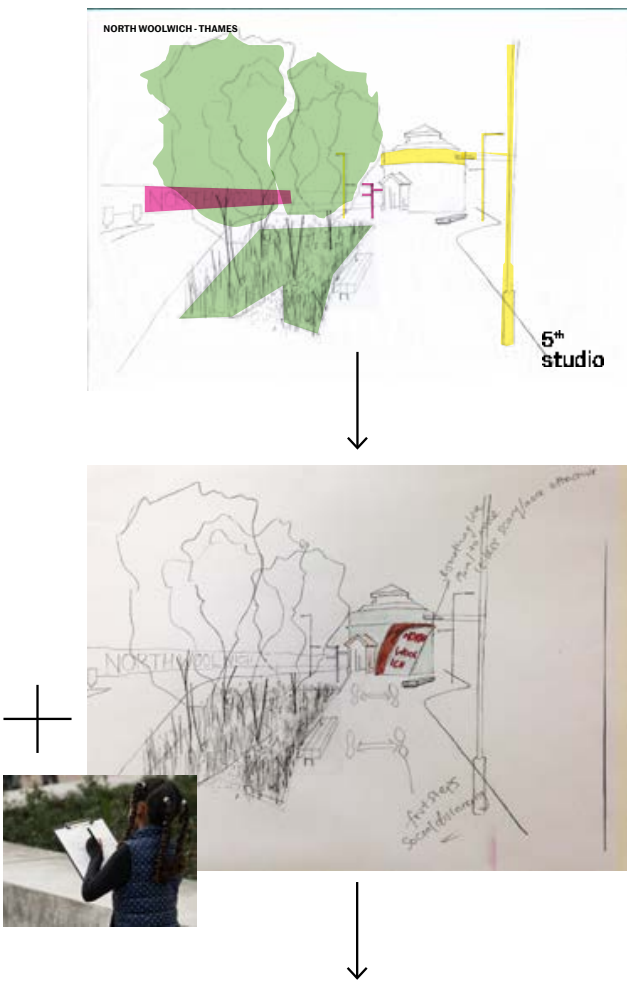
- Priority locations for seating and furniture, proposed location for a community garden included in Masterplan, proposals for community noticeboards or moveable meanwhile planters.
- The principle of Play for all ages integrated into Landscape elements supported by functional Lighting and Wayfinding.
- Threshold mats to gather street furniture and support dock edge wayfinding to encourage different uses and enjoyment of the water.

### Lighting

- Different, complimentary lighting solutions through a baseline and accent palette to meet different needs at different times for example to encourage activity or safety at night, while preserving tranquillity and avoiding light pollution.
- Strategy for consistent lighting around dock edge with feature lighting to key objects and buildings.

### Wayfinding

- Proposals for interactive maps and information boards. Local locations of severance between amenities, and for signage improvements.
- The application of colour to the Stitches and incorporating sensory elements into Wayfinding to enhance the experience of different places.
- The principle: 'Inclusive' includes key moves to design with the input of communities to optimise mobility and access to amenities.



- Community art including children.
- Around the area of the foot Tunnel entrance there is too much focus on the cars, there is not enough space for pedestrians and cyclists.
- When you come from the South side and exit at the North side of the tunnel there is no signage so if you don't know the area you won't know where to go. Should include sign to the park.
- It currently feels too open and too much concrete, it does not invite you to go there.
- The area outside the tunnel should be better landscaped with benches you can sit down on and welcoming trees, plants, flowers so that people could hang out there.
- Imagination, creativity, playfulness and learning opportunities.
- Could have some shrubbery outside the tunnel.



# Stakeholder and Community Engagement Outcomes

Community design principles  
The Draft Community Design Principles outlined on this page summarise the analysis of communities’ input. By doing so under broad themes, they can be used to support a consistent experience for all Royal Docks locals and visitors, and to promote an accessible and inclusive public realm. The intention is for these principles to be applied across the Design Guides and used by all stakeholders to inform their approach to Public Realm design and delivery.

The next steps will be to work with communities and stakeholders to consolidate these principles, plan ways to put them into action and build accountability around them. This may include community design review panels or other occasions when evidence of a process and an adherence to design principles might be required.

Two interconnected values underpin the principles, reflecting Newham and London’s current policy approaches. These are repeatedly evidenced through community feedback:



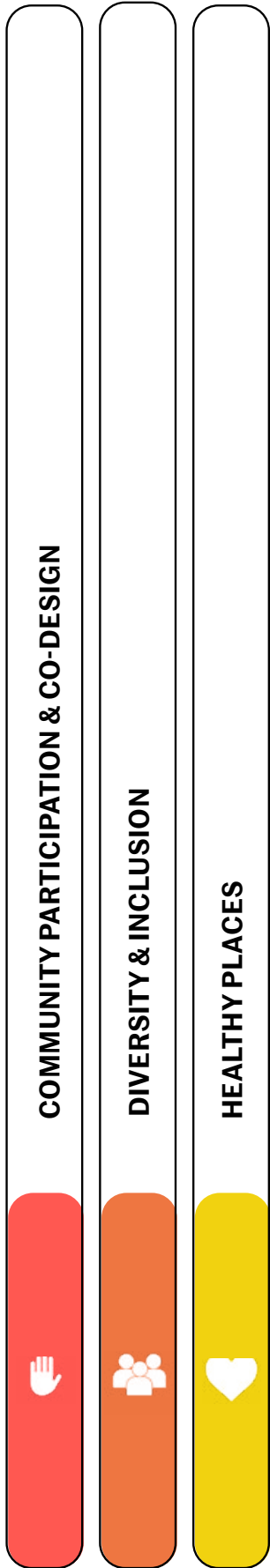
**Diversity and Inclusion**  
The Royal Docks Public Realm aims for inclusive environments that can be enjoyed by everyone: existing and new residents, people of any ability, age, gender, sexual orientation, race or faith. High levels of public access and varied uses by diverse groups at different times aim to promote integration. The Accessibility and Inclusive Design Guide provides technical guidance on these issues.

**Community participation and co-design**  
This is a cross-cutting principle which should be applied alongside other principles. The appropriate level (from information and consultation through to co-design) and form of participation should be planned with communities well in advance of any project, with clear objectives and proper resources. Ensuring a multi-stakeholder commitment to this longer-term process, leaving room to flex and evolve, can yield greater place-making outcomes for all.



**Healthy places**  
Public Realm can promote physical and mental health, and general well-being. When a variety of inter-connecting principles are applied to place-making, it can ensure that the projects enable social connection, leisure and relaxation; facilitate mobility and active travel; enhance community care and safety; connect to amenities and economic opportunity; and are clean, well maintained and ecologically sensitive.

## Community Design Principles



- Ecology**  
Promote nature-based solutions and low-carbon communities that reduce pollution and waste. Encourage biodiversity, be climate change adaptive, promote people-nature connections.
- Play and active design**  
Integrate play and physical activity opportunities into the public realm for people of all ages with formal, informal and creative measures. Support active travel, to ensure accessibility and awareness.
- Sensory design**  
Incorporate pleasant sensory experiences (sight, sound, smell, touch) throughout the public realm, including art and creative elements to enhance tranquillity, attractiveness, imagination and interconnection with the surroundings.
- Living heritage**  
Bring the area’s industrial, maritime and socio-cultural history and infrastructure into play in the public realm to inspire curiosity and feelings of belonging at a human scale by creating inviting spaces and learning opportunities.
- Water connection**  
Consider water as public space, enhancing access for different users with different needs. Encourage views and wayfinding, and support its role in ecology and climate resilience.
- Flexible use: multi-functional and adaptive**  
Maximise opportunities to enable the free use of space by the public. Consider the increased and multiple use of space for social and livelihood activities at different times of day by building in adaptivity across strategies.
- High streets and amenity centres**  
Multiply civic links to local centres with accessible high streets which support daily needs and livelihoods and provide opportunities to meet, talk and celebrate.
- Hyper-local networks**  
Enable and enhance existing networks of care, socio-spatial connections, local mobility, and the exchanges of goods, services, support and knowledge.
- Community stewardship**  
Emphasise and encourage community knowledge and action to maintain and curate public and green space; to drive low carbon solutions, ecological stewardship and local strategies for community resilience.

# Innovation at the Royal Docks

With so much unconstrained space, and a blend of public and private sector investment, the Royal Docks are ideally positioned as an urban proving ground, as a site of innovation and experimentation for London. This was identified in the 'Economic Purpose for Newham' report, 2019:

**“The Royal Docks Economic Purpose is proposed as a nationally significant hub of enterprise, employment and culture, recognised as a testbed for social, environmental and technological innovation”**

This is why we want to work to establish the Royal Docks as a place where innovation is encouraged, and as a place that provides learning for policy makers, practitioners, and communities across London and beyond.

The Public Realm Design Guide has set out key opportunities for this innovative culture, from Smart City investment for creating Healthy Streets, to emerging thinking on new economies, energy and environmental sustainability. The approach supports The Mayor of London's 'Good Growth' approach and the London Borough of Newham's focus on Community Wealth Building.

## Why?

There is a growing demand for urban innovation in cities. In the Royal Docks we have an opportunity to use new technologies and innovative design approaches to the public realm to address challenges such as environmental change, infrastructure provision and the impacts of growing population density and inequality. Some of these are listed opportunities are listed below.

## Climate change

The biggest current challenge facing cities is how to mitigate the impact of climate change. This includes how to address flooding risks, provide shade and shelter to improve comfort levels in extreme weather (urban heat island effects) and to address biodiversity loss.

## Infrastructure provision

- Provide renewable energy sources.
- Encourage the use of active travel modes.
- Circular economies.

**Innovation**

Refer to these markers throughout the Design Guides for references relating to innovation, precedent projects and references.

## How?

All stakeholders, including Public Realm Designers, have the potential to create new solutions and to use existing infrastructure in new combinations to address these challenges.

Engagement with communities, other innovators, institutions, and research projects, could create funding opportunities and capitalise on knowledge and skills to create innovative solutions. The UK has a strong research base and world-class higher education institutions as well as capabilities across business sectors for urban innovation.

Tactics include utilising digital technology, testing/prototyping, spatial design (including co-design) and physical infrastructure:

## Digital technology

- Engaging local communities by using data, for example by monitoring air quality, and providing visual data to illustrate how contaminated groundwater is being monitored.
- Data collection in the public realm, from sensors, digital street furniture, advertising and other new technologies embedded into street furniture.
- Co-design/community feedback loops using community media, audio-visuals and social media.
- Digital signage with real-time information across the area providing live data on transit arrival and departure times, walking times, nearby transit routes, and the availability of shared bikes, cars and scooters, all making it easier for people to navigate their sustainable transport options.
- Mobile infrastructure - small cell data integrated into street furniture.

## Testing/prototyping

- Learning lessons from similar projects in other locations, testing approaches that can in turn be rolled out elsewhere in London and using demonstration sites to trial new urban solutions.

## Spatial design

- Dockside parks and proving grounds - Temporary landscapes and innovation expos providing places for testing resilience, species, management - a Landscape Laboratory.
- Sustainable Urban Living & Low Carbon Neighbourhoods - Cycle parks, market squares for stalls selling locally produced food and goods by local creatives.
- Urban Ecology/ Food production - Grow and produce food, providing access to locally grown food and a means of bringing the public closer to how food grows and familiarising them with regionality and seasonality. There are also opportunities for farm to table enterprises which can sell either to restaurants or to the public at a farmers market.
- Sustainable urban green spaces - Restored green spaces from brownfield sites, soils and bioremediation and the recovery of contaminated sites by understanding interactions between soil conditions, wasteland vegetation, pollutants, and their exposure and toxicity to animals and people.
- Living Water City - Floating Ecosystems and islands provide refuges for nature and wildlife. 'Active islands' can treat urban waste water and run-off. Soil-less agriculture, floating polytunnels, aquaponics, flora that is planted (reeds, lilies, floating forests), growing (greenhouses heated by thermal rays), or oxygenating the water (aquatics, emergents, marginals).
- Shade and cooling - Mitigating the urban heat island effect through technology and the use of landscape interventions. Urban greening through vertical planting, urban trees, green roofs and rain gardens helps reflect light and evaporate moisture, in turn reducing ambient temperature of local microclimates. Cooling and permeable paving solutions, with lighter pigments and aggregates in materials, reflect more sunlight.
- Water management - Sustainable Drainage Systems (SUDS)/surface water run-off, access water and energy consumption data. A strategic approach to SUDS is vital for managing surface water-run off volumes and flow rates, mitigating flooding and improving air quality. SUDS can enhance biodiversity, and create diverse and engaging places for people through integrating social functionality with street furniture and playable public realm elements and wildlife. Areas of reed beds introduced at the dock edge takes surface run-off water and cleans it prior to its entry into the dock basin.

## Physical infrastructure

- Streets - Electric charging, wireless electric charging technology for buses and utilities.
- Future Shipping - The Thames and its Docklands have a great history of shipbuilding and marine innovation. There is huge potential for research and development of new forms of zero carbon shipping, powered by wind, hydrogen, and the water itself.
- Airport - Re-wilded infrastructure. Tree planting at London City Airport using robust and non-bird harbouring tree species such as, establishing a birch forest.
- Lighting - Central control system. Granular control, reducing lighting levels, control for events, Internet of Things (IoT) integration. Incorporating advanced lighting sensors to account for ambient / spill lighting from private sources.
- Structures - Integrating technology into lighting columns; power/water/wifi/electric vehicle charging.
- Power generation - Kinetic pavements and on-grid solar lamp columns might be considered: large open areas, such as the ExCeL car parks, could be pilots for solar columns. Using the dock water as an energy source, this could power a circulation system to keep the water in Victoria Dock oxygenated and fresh.
- Encourage economic growth - Empower local businesses and connect communities. Attract hubs for innovation and technology by providing vibrant outdoor environments and removing the need to use cars. Create socio-economic opportunities for job and skill creation particularly around digital upskilling.



The design guide proposals put innovation at the centre of public realm improvements. The dock edge worked example above illustrates network infrastructure integrated with wayfinding elements, biodiverse urban planting in the basin, a lighting environment controlled by a 'central management system' and the physical and cultural activities made possible by providing greater access to the water.



# Building on the Framework

The Framework, published in Spring 2020, sets out the vision for the public realm within the Royal Docks. This vision is based on an overall philosophy - Occupy the Docks - as well a series of guiding principles focused on places, the water, movement and landscape.

### Occupy the Docks

The overarching principle, ‘Occupy the Docks’ refers to an ambition to make the Royal Docks a more varied, rich, and well connected place - a successful piece of city.

The Framework identifies two key tactics that should be employed in order to achieve this:



The Framework



Above: A series of pontoons in Amsterdam North create a more sheltered and welcoming environment to encourage access to the water space of the docks.



Above: Similar investment in the Isle de Nantes, France have established this former dockland as a centre of excellence for animatronics and cultural event structures.

### 1 - Transform infrastructure into city!

For the Royal Docks to be successful as part of the diverse and international fabric of Newham it needs to be stitched in and connected. The intervention proposals within the framework are essential to bind this place together successfully.

The high level proposals within the Framework set out a path to ensure that the Royal Docks flourishes. They guide how historic infrastructure barriers can be overcome to develop as a new multi-layered joined up part of London: distinctive, active and inclusive.

### 2 - Foster a culture of innovation!

With a range of spaces that (compared to other places in London) are relatively unconstrained, and a blend of public and private sector investment, the Royal Docks is ideally positioned as an urban test bed - as a site of innovation and experimentation for London.

The Framework sets out some key opportunities for this innovative culture to create Healthy Streets, to accommodating emerging thinking on new economies, energy and environmental sustainability.

## Key issues identified within the Framework



### Access to green open space

The area suffers from poor access to green open spaces. The provision of new green spaces, as well as improving access to existing spaces, is an underpinning strategy of the framework.



### Inconsistent approaches to lighting and wayfinding

The presence of a number of redundant elements across the docks severely hampers effective lighting and wayfinding, as well as creating a cluttered street scene. At night a number of key spaces and landmarks are not adequately lit.



### Under-use of the dock water

Using the water for recreational use is currently difficult, due to a number of restrictions. There is also a lack of structures that bring you to the water.



### Poor connectivity and local severances

The urban structure is dominated by east-west movement (along road corridors like North Woolwich Road and Royal Albert Way, and the DLR lines). There are significant severances due to large impermeable areas like the docks, airport, rail corridors, and industrial sites.

The following pages set out the Framework’s ‘Guiding Principles’ that respond to the issues listed here. These principles set out the strategic moves that should be taken to overcome the issues in the Docks to make it a successful and integrated piece of the city.

# Water and public spaces

## Accessible and sustainable spaces

The unique water spaces of the Royal Docks are the area's biggest asset and main characterising element. The water will provide a focus for the development of the area. The role of the dock water should be re-imagined in order to activate the dock edges and bring the water space into public use.

## Active Water Uses

Active water uses, ranging from destination experiences to club sports, should be encouraged while also maintaining moments for the public to get close to the water, pause, and freely enjoy this expansive and unique natural feature.

## A Publicly Accessible Dock Edge

A publicly accessible dock edge should be developed in a manner which is coherent and clear across the various land ownerships that the dock edges sit within. Access to the dock edge should be supported by a network of routes with utility and recreational value – connecting communities and green spaces.

At the dock edge there should be a focus on evidence of the area's cultural heritage, embracing the unique cultural and heritage setting of the Royal Docks, including visible maritime objects, and projects such as illuminating the dockside cranes.

## New Routes

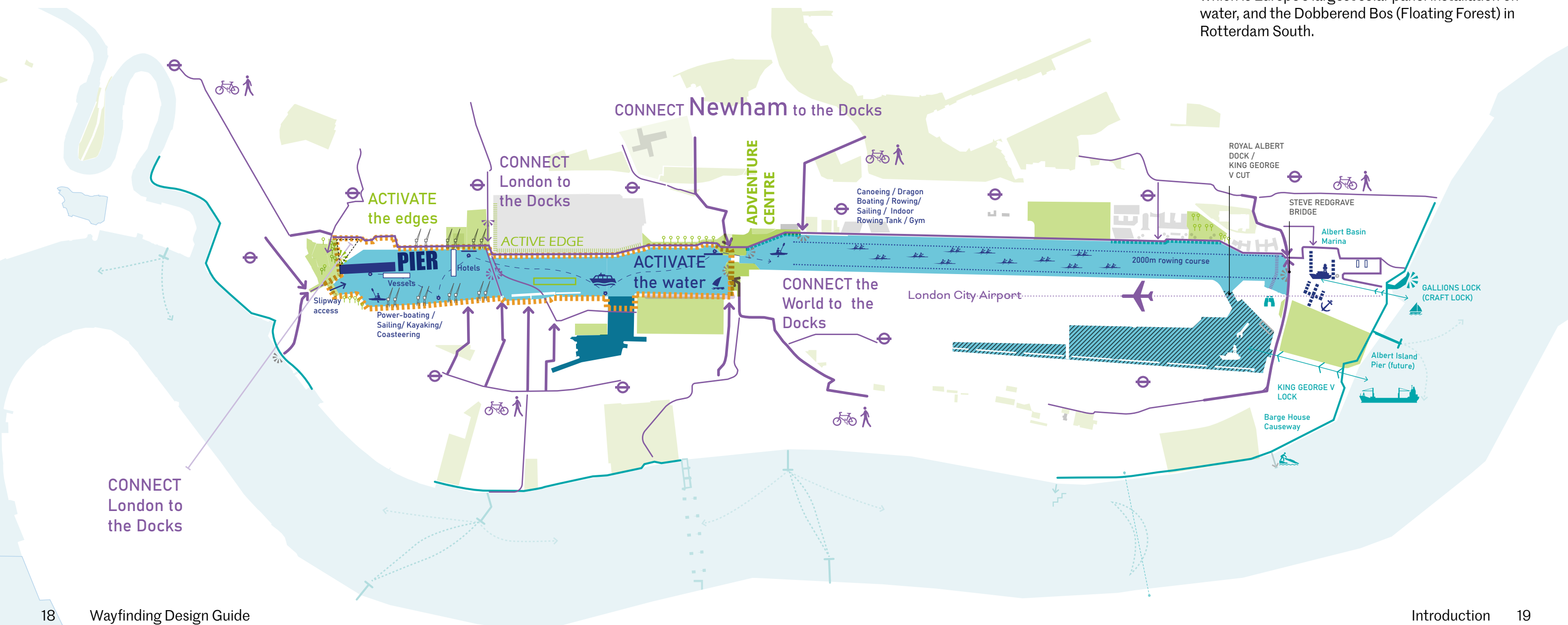
New routes including a continuous route around Royal Victoria Dock, would release the potential for the Royal Docks to become an amenity for local communities. Improved access to the dock edges with walking and cycling routes could provide health and social benefits.

## Creation of Safe, Active, Popular and Inclusive Spaces

The Public Realm Framework supports the creation of active, popular and inclusive spaces in and around the Royal Docks which can generate revenue, all the while supporting the broader business case for investing in the public realm. Uses around the dock edges should cement the reputation of the Royal Docks as a place which is enterprising and industrious, supporting education and training for Newham residents.

## Enhancing the Dock Landscape

The blue landscape should be adjusted to embed climate adaptation solutions. Examples include the Solar Farm on the Queen Elizabeth II reservoir, which is Europe's largest solar panel installation on water, and the Dobberend Bos (Floating Forest) in Rotterdam South.





# Places

## Active, diverse places

The Royal Docks is home to a diverse range of places, each with their own distinctive activities and uses. Celebrating this diversity within a coherent, well-designed public realm will be key to the success of this area.

The Public Realm Framework identified a series of distinctive places around the Royal Docks which are characterised by current or future proposed activity. These places have a particular economic or cultural offer operating at global, national and local scales.

Proposed new spaces should be designed to meet the requirements of the area and be designed to support a range of uses – for example to meet anticipated cultural programming. They should be ambitious, relating to the acknowledgment that there is difference across the Royal Docks – the area is made up of a set of ‘distinctive places’.

## Royal Victoria Dock West - A Destination

The western end of the Royal Docks offers a series of visitor attractions and public spaces that sit within the wider sub-regional scale of East London.

This place is a focal point for the regional visitor economy within the Royal Docks with the potential for new interpretation and cultural spaces. Public spaces around Royal Victoria Dock West should be designed to accommodate large numbers of people to allow for the proposed cultural programming in this location.

## Royal Albert Dock - International Enterprise

Royal Albert Dock is a centre for international business and enterprise capitalising on its proximity to London City Airport. Significant change is proposed here, the first phase of commercial space within Royal Albert Dock provides 460,000 sq ft of office space.

## North Woolwich/ Canning Town/ Custom House/ Beckton - Local Communities

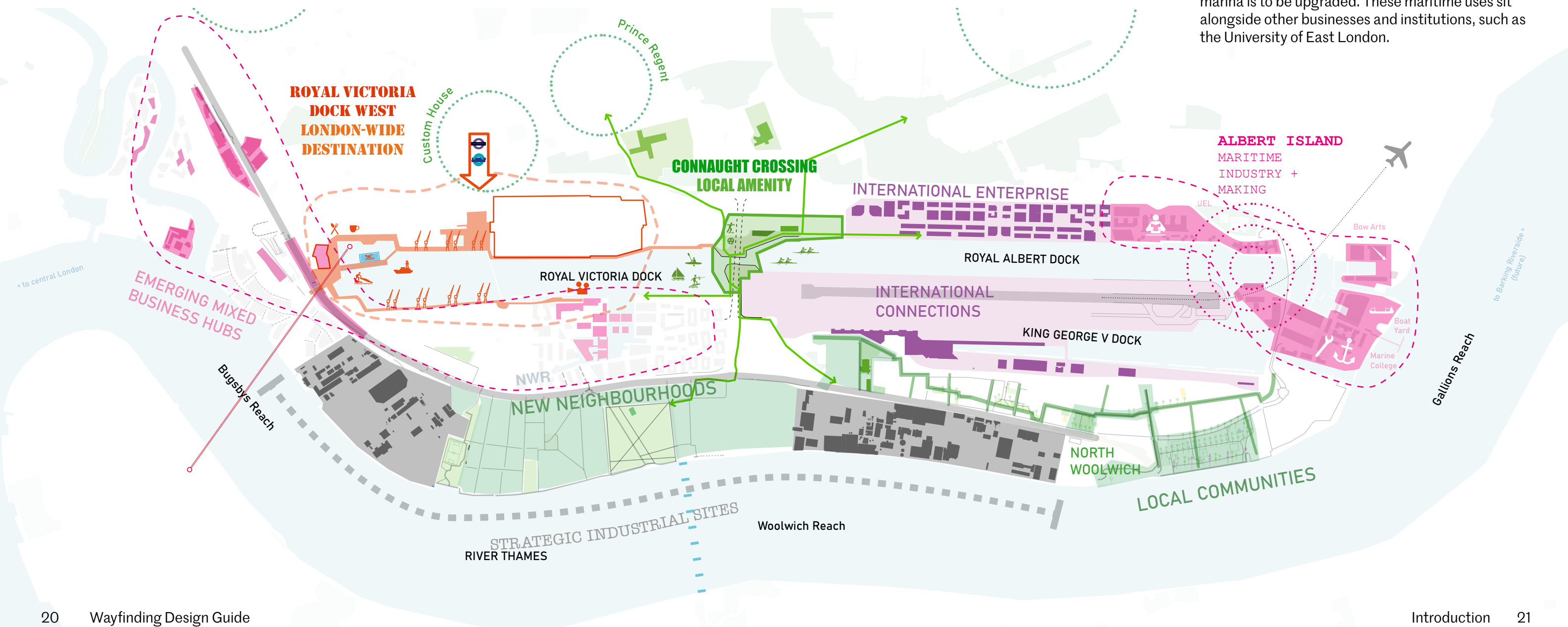
New residential developments in the Royal Docks will create new communities sitting alongside the established communities in North Woolwich, Canning Town, Custom House and Beckton. Activity to support local people in these locations must be physically, economically and socially connected.

## Connaught Crossing - Local Amenity

Connaught Crossing is one of the few places where you can cross the Royal Docks from north to south, connecting Newham, via the ‘green bridge’ over Royal Albert Way down to North Woolwich to the south. This axis supports local community activity in spaces including the Asta Community Hub and the Royal Docks Academy.

## Albert Island - Maritime Industry and Making

Albert Island at the very eastern edge of the Royal Docks is a place with a focus on industry and making. A new shipyard is proposed which will service and repair Thames riverboats, while the local marina is to be upgraded. These maritime uses sit alongside other businesses and institutions, such as the University of East London.



# The Stitches

## Connective landscapes

Through improvements along key routes around the Royal Docks – the Stitches – we can overcome how industry, roads and the water have historically created severances between the areas that make up the Royal Docks.

Through improving these areas and signposting key crossing points, a generous public realm can be established, ensuring that all community members and visitors are supported in accessing the area. This broad network of connections will also unite the water spaces of the Royal Docks, bringing them together with the growing communities to the north and along the river.

## Canning Town to the Docks

By connecting the Leaway to the Royal Docks, via Canning Town and eventually the Limmo, the Lea River Park's green connections can be enjoyed by more people. Through completing this route, a traffic-free connection between two of Newham's major growth areas can be achieved, establishing off-road links to the Queen Elizabeth Olympic Park and Canary Wharf.

## Custom House to the Thames

Centuries of infrastructural works at the edges of the docks have created significant barriers to movement between the communities around Freemasons Road, the Docks, and the River Thames. Overcoming these severances will lead to a diverse series of connected spaces and greater access to cultural and recreational activities around the docks.

## Connaught Crossing

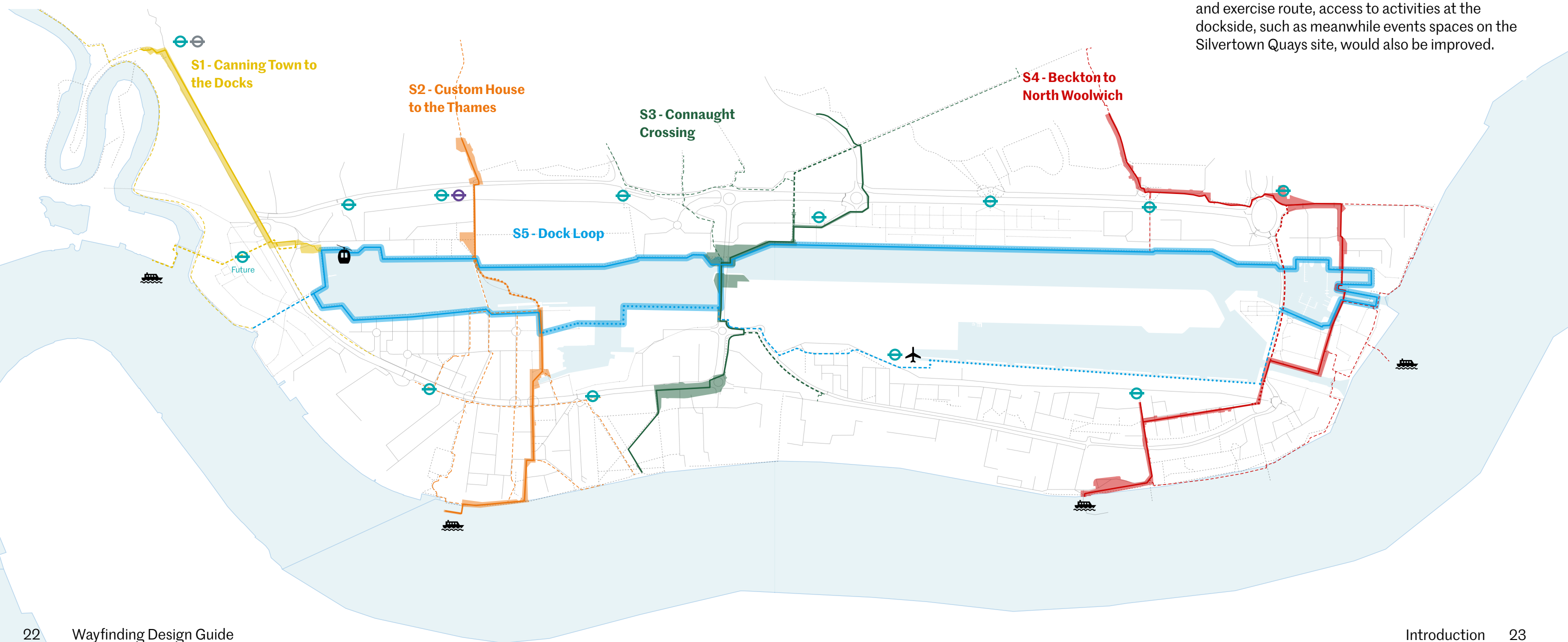
An improved offer of leisure and recreational activities, as well as connectivity improvements which seek to humanise the scale of existing road and DLR infrastructure, can reconfigure the Connaught Crossing as an 'armature for local amenities' and a destination in its own right.

## Beckton to North Woolwich

Poor quality infrastructural environments such as the Steve Redgrave Bridge, Woolwich Manor Way and Factory Road hinder a strong sense of place, north and south of the docks. Local connections and public realm improvements could ensure that these two established communities have a renewed connection to the Thames and its maritime heritage.

## Dock Loop

Establishing a continuous, well-signed walking and cycling loop around Royal Victoria Dock would vastly improve people's connection to the water. As well as providing an opportunity for a fitness and exercise route, access to activities at the dockside, such as meanwhile events spaces on the Silvertown Quays site, would also be improved.





# Character areas

Biodiverse landscapes  
The landscapes of the Royal Docks are heavily informed by the area's history. Across this vast area, underlying latent landscapes can be revealed and celebrated in order to improve cohesion and identity, as well as providing high quality public spaces.

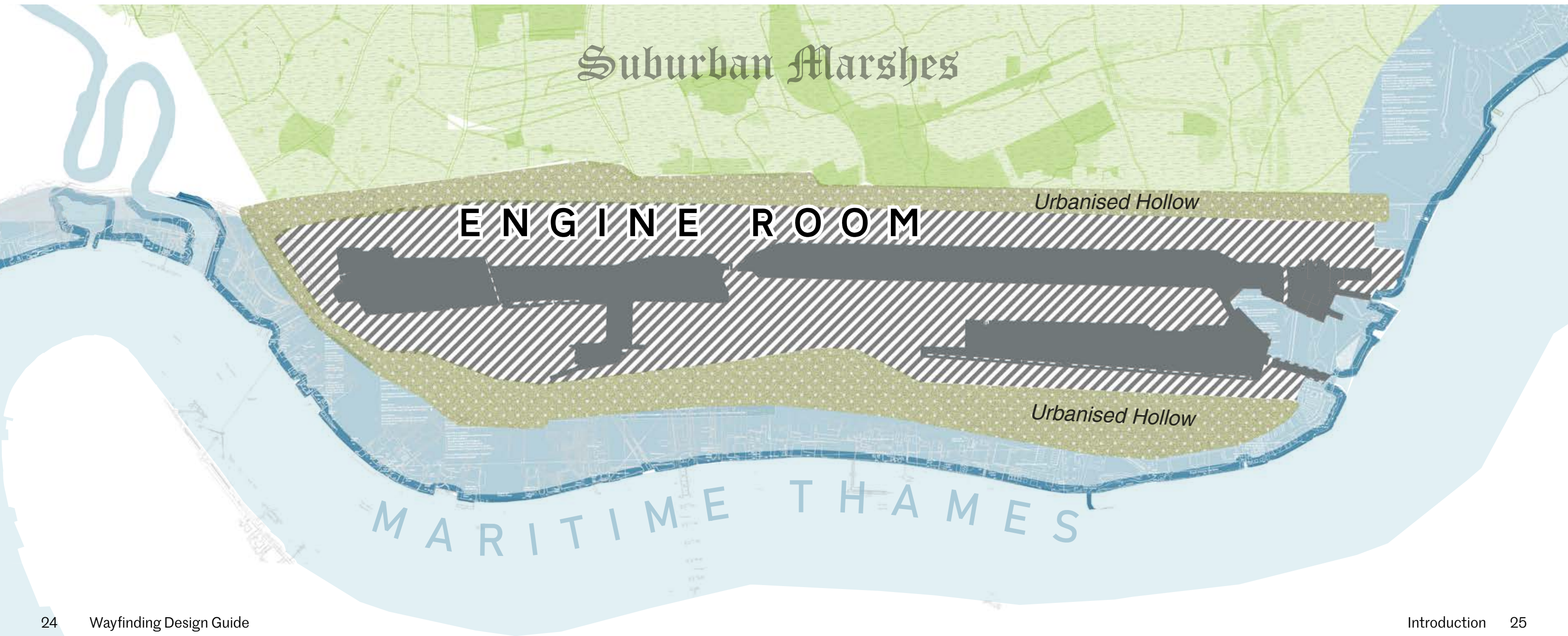
These historic characteristics can inform how new and existing public realm and landscapes can evolve or be adapted. These hints back to history are a tool to support a coherent landscape attitude across the Royal Docks, and helps us create a common atmosphere that is scalable and rooted in place.

Suburban Marshes  
This landscape area is defined by the former Plaistow and East Ham Levels: large expanses of former marshland which were drained and managed originally to create arable land, and from the early 19th century, a sequence of residential areas. Despite the distinctly suburban developments of the 20th century, the remnant traces of the former marshlands are still evident. Green spaces, drainage channels, allotments, and mature trees provide a unifying character.

Urbanised Hollow  
The embankment of the docks and the raising of the river edge against flooding has created a low point or 'hollow' between the Thames and the dock basins and again north of the dock basin on the boundary of the residential area.

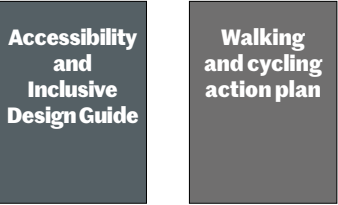
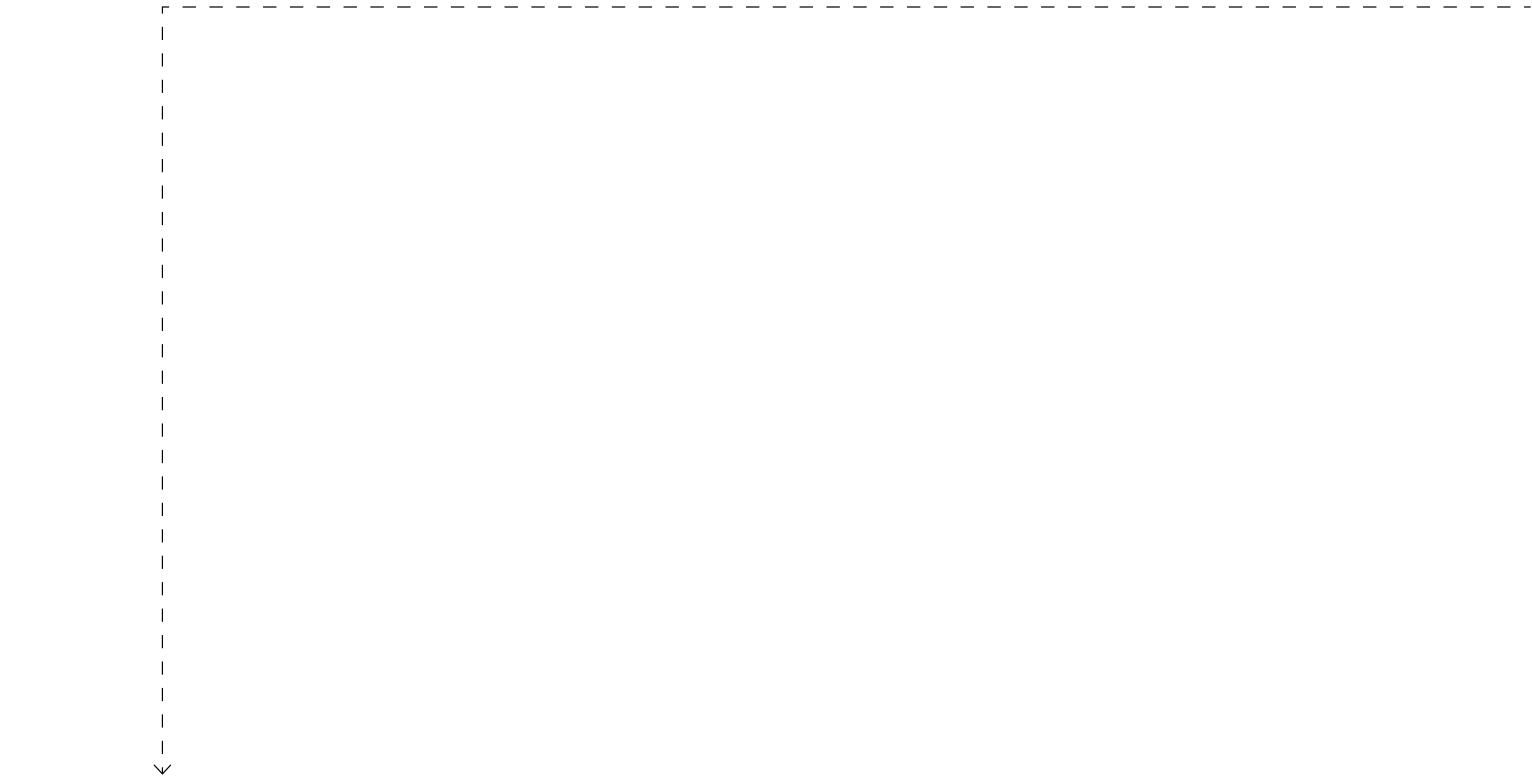
Engine Room  
Developed through the 19th and early 20th Century, the Royal Victoria, Royal Albert and King George V docks are the largest area of man-made impounded water in the country. As such the docks themselves are a singular and purposeful landscape, equivalent to approximately the area of Heathrow.

Maritime Thames  
The Thames edge suffers from a common condition seen across London, where the need to ensure valuable wharves and riverside warehouses are kept secure has physically prevented the public from accessing the riverside. Access is generally halted one block back from the riverside, and tantalising glimpses of the Thames are given occasionally between buildings that front the water. The character of the Thames edge is defined by its exposure to the river, creating a strong tie to the changing tides, weather, wildlife and long views out to wider London.



# How to use this Guide

This guide is structured to give users a broad range of information to assist them with designing public realm in the Royal Docks. The diagram opposite shows how users can engage with the guides in a simple and effective manner.



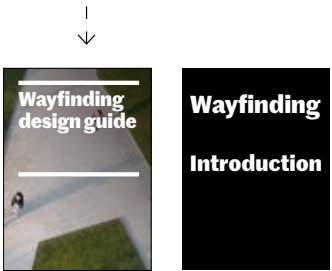
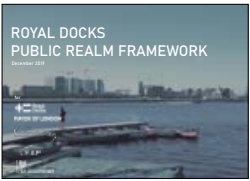
\*to be published after the Design Guides

What are the accessibility standards?

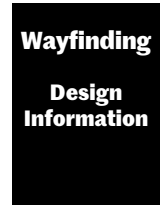
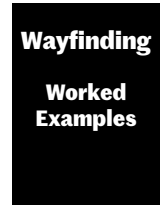
What are the planned improvements for cycling and walking?

The **Accessibility and Inclusive Design Guide** will set the benchmark for accessible design across the Docks. Specific interventions to assist walking and cycling will be documented in the **Walking and Cycling Action Plan**.

## Using the Design Guides



\* refer to **Accessible Design Standards** and **Walking and Cycling Action Plan**



What is the overall vision for public realm?

What is the vision for my area?

What key principles should I consider when producing designs for my site?

By first referring to the **Public Realm Framework**, users can understand the overarching vision for transforming the Royal Docks. The **Design Guide Introduction** then presents a refined set of principles which should be used when designing in the area.

How do I know which palette(s) to use on my site?

What elements make up each palette?

The **Masterplan** chapter provides a spatial overview of the proposed palettes, as well as other key considerations. Users will be able to quickly locate their site, and then refer to the **Palette** chapter to view the range of suggested elements for their specific location.

How do I use multiple elements from the palettes together?

How would they look on site?

The **Worked Examples** chapter will demonstrate how the palette elements can come together, using annotated rendered views from diverse locations across the Docks.

What are the technical specifications for certain elements?

How will I maintain certain elements?

Maintenance advice and technical specifications for key elements from the **Palette** chapter will be documented in the **Design Information** chapter.





The following chapter describes the wayfinding masterplan in two parts; a series of wayfinding design principles and the overarching wayfinding masterplan.

The **wayfinding design principles** highlight the thematic opportunities present across the Royal Docks. The detailed design of all interventions should be developed with reference to current best practice and these design principles.

The **wayfinding masterplan** has been developed to:

- Emphasise the unique positive characteristics of the docks' culture and heritage.
- Steer the delivery of a coherent public realm across the Royal Docks.
- Reinforce the legibility and utility of the stitches which connect the area's communities.

This chapter should be read in conjunction with the accompanying lighting, landscape and accessibility and inclusive design guides which make up the Royal Docks public realm designers' pack. Illustrative views of how the masterplans inter-relate are illustrated in chapter 4, Worked examples.

## 2. Wayfinding Masterplan



# Design principles

## 1 Of the Docks

The area's rich cultural and built heritage provides opportunities to creatively sign and emphasise these landmarks and stories. Themes include the development of the Docks, maritime trade, industry, food and communities.

Work has been progressed already in the development of the Royal Docks brand design guide. This draws upon the nautical history of the Docks and can act as a reference point for the development of signage across the Royal Docks.

### Key moves:

- Wayfinding should be used to enhance the character of a place, as well as for navigation.
- Co-opting existing structures in the docks to enhance their identity.
- Creatively signing landmarks to reveal their history, and their relevance today.
- Reclaiming structures for wayfinding.
- Curating nautical objects across the territory.
- Bespoke wayfinding elements that contribute to the identity of the Royal Docks.
- Incorporating Royal Docks branding in key locations.

## 2 Clear

Making sure the wayfinding strategy is clear, inclusive of different communities and does not create a confusing visual environment. Interventions should also involve clearing away the clutter of redundant and unnecessary signage. The particular needs of all members of society should be taken into consideration across all stages of the wayfinding design process.

It is important that signs are only installed where necessary and does not add to clutter or visual noise. Careful consideration should also be given to the risk of over-signing environments – allowing for individuals to take in the landscape itself rather than having a sign interpret it for them.

### Key moves:

- Remove redundant signage.
- For basic navigation, complete the Legible London network in the Royal Docks.
- Use a diverse range of elements, asides from just signage, to assist with wayfinding and place-making.
- Propose a palette of bespoke wayfinding elements that can be used sparingly, with high impact.
- Ensure that necessary information (such as health and safety warnings) can be successfully integrated with proposed wayfinding elements.

## 3 Inclusive

Throughout the design process teams should be engaging with communities to understand specific opportunities for co-producing projects with community groups, allowing the design work to respond to particular community initiatives and requirements.

### Key moves:

- Use wayfinding workshops to enhance common understanding between communities, stakeholders and designers.
- Identify how wayfinding can support existing communities and activities in the Royal Docks.
- Initiate 'joyful testing' interventions that explore long term strategies.



A



B



C



D



E

- A Royal Docks Adventure Centre, London
- B Darling Harbour, Sydney
- C Franklin Wharf, Hobart
- D Royal Docks signage, London
- E Forecast by Barber & Osgerby, London



# Wayfinding masterplan

The wayfinding masterplan is split into three layers illustrated in the diagram opposite. These layers work together to balance continuity to the wayfinding across the Royal Docks while introducing variation based on the Stitches and the area's unique characteristics. When combined, the aim is to create a legible public realm with an identifiable yet varied approach to wayfinding.

Accent wayfinding palette  
- Identifiable connective stitches

Wayfinding should be effectively used to highlight the 'Stitches' at all times of the day. To achieve this an accent palette has been prepared for these routes.

This will require co-ordination across multiple land owners, but will be key to ensuring that the key routes across the Royal Docks feel legible and socially inclusive.

The accent wayfinding palette sets out the opportunities for colour, graphics, and curated objects to identify the individual Stitches, and further strengthen their identities.

Baseline wayfinding palette  
- A legible wayfinding strategy

Across the Royal Docks area there is the ambition to create a more coherent wayfinding strategy which supports walking and cycling. Key to this is the continuing deployment and updates of Legible London products which form the baseline wayfinding palette.

This palette follows best practice approaches as demonstrated across London.

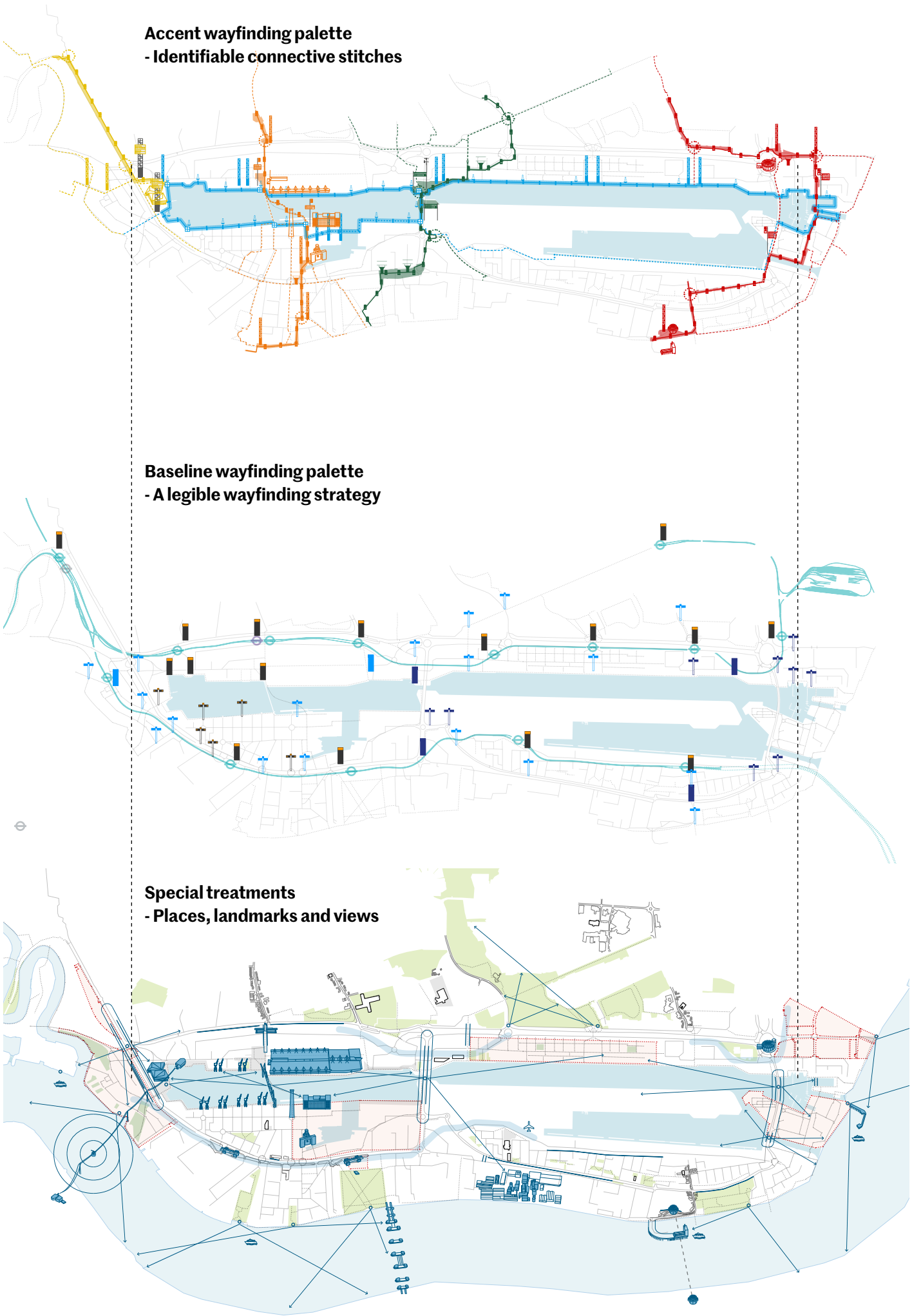
Refer to the chapter 3, Palettes, for detailed guidance on the masterplan strategies >

Refer to the detailed masterplan appendices for the locations of public realm proposals >

Special treatments  
- Places, landmarks and views

There are many local landmarks around the Royal Docks, ranging from community centres, built heritage, to recent infrastructure and the dock water itself. During the day these act as useful structures for navigating the area, with many identifiable from a long distance.

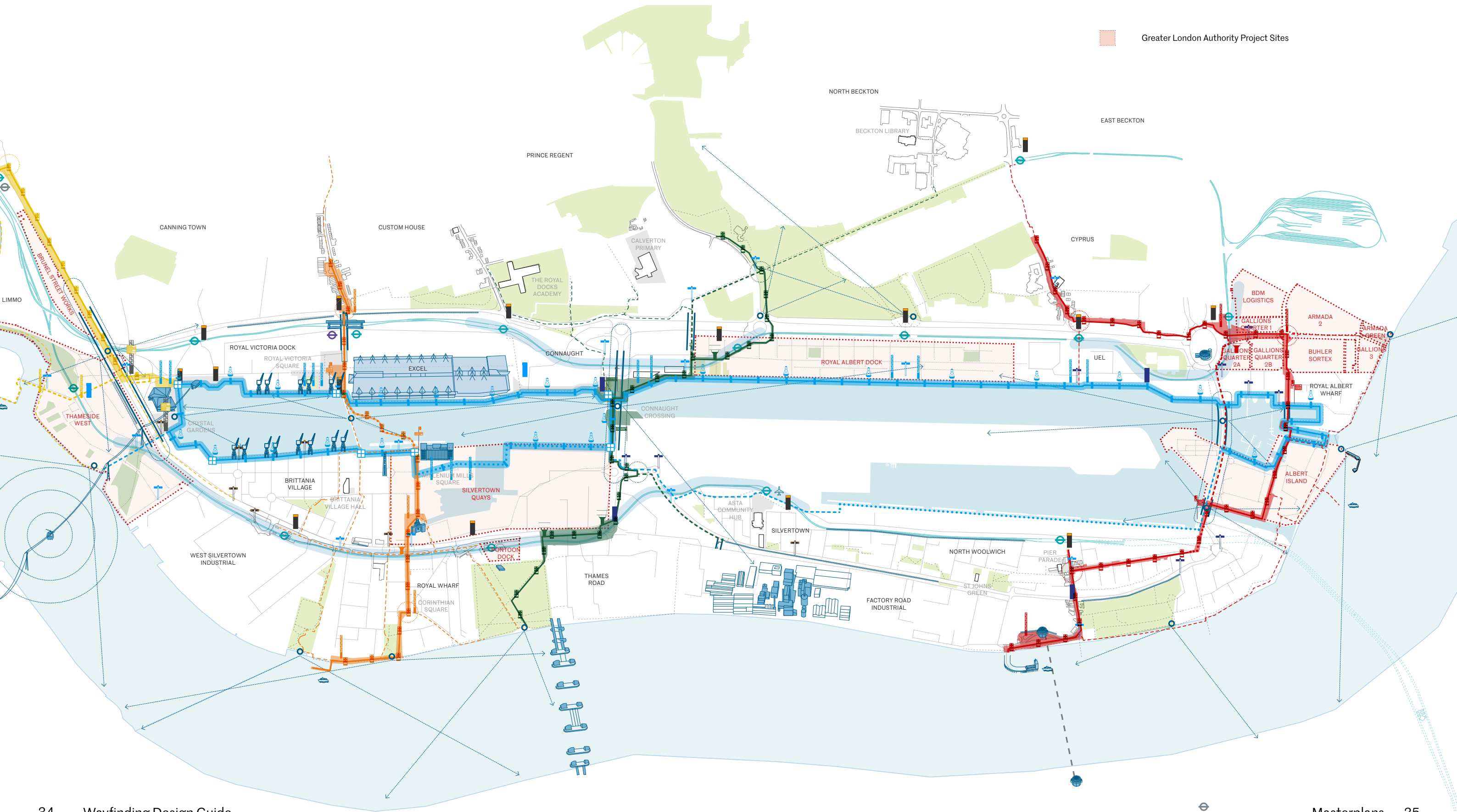
Alongside a wayfinding role these landmarks also offer the opportunity to create points of focus for the communities around them. Highlights should be created through special interventions around landmarks, and the creation of long views, to help orientate the public as they move around the Royal Docks. At all times, any new developments need to consider how it affects existing landmarks and avoid any negative impact.



# Wayfinding masterplan

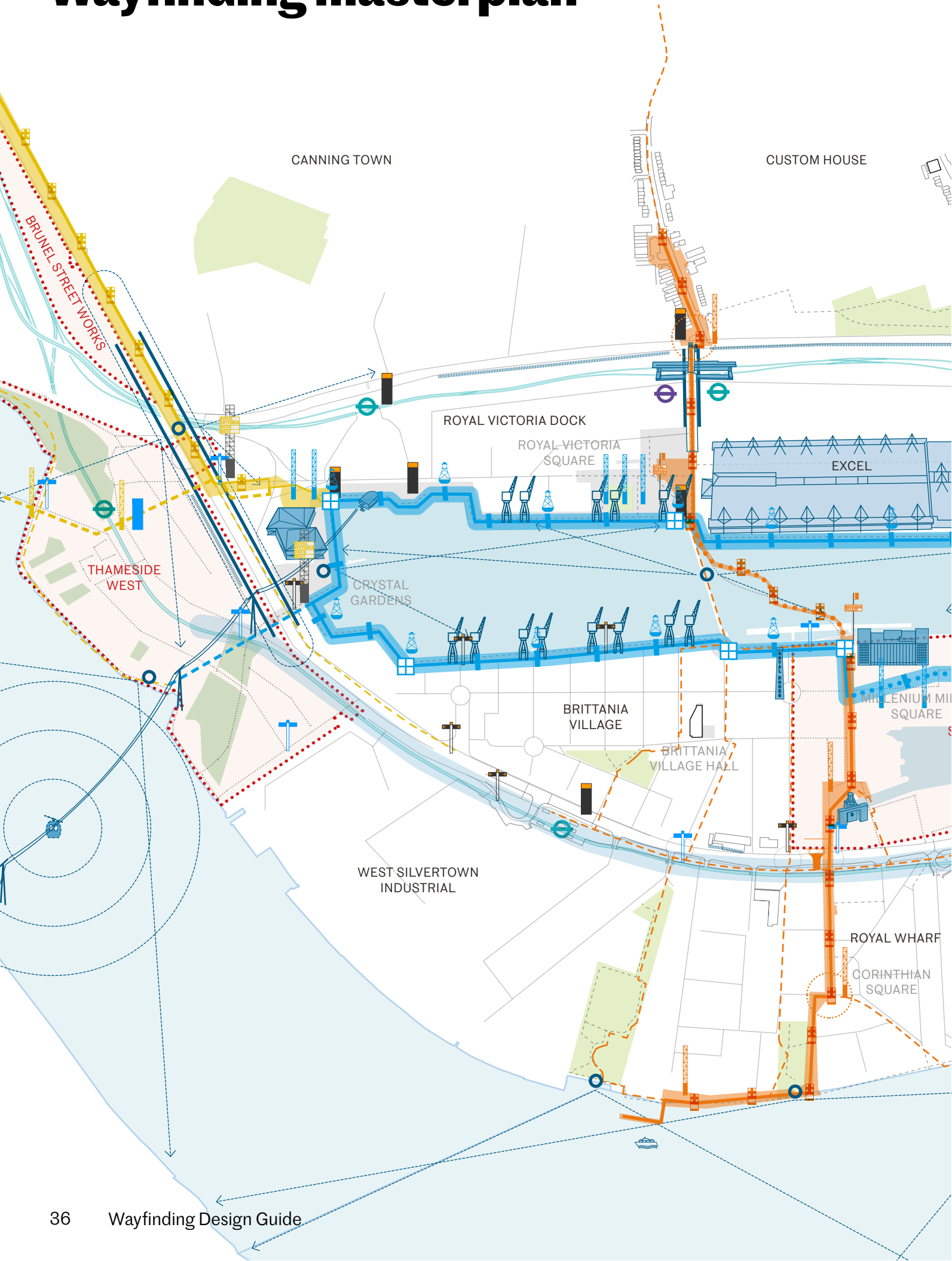
The masterplan below illustrates the range of approaches for improving the wayfinding within the Royal Docks. The drawing shows how the palettes are strategically deployed across the site with indicative locations of individual palette components.

The following pages provide zoomed-in versions of this drawing, so that users of this design guide can identify their site and see which palettes and strategies should be employed in their location.





# Wayfinding masterplan



## Area description

A number of key stitches bisect the docks, with connections to the rivers Thames and Lea, the local centre at Custom House and the existing and emerging communities to the south of the dock water. Many grand historic structures and spaces characterise the area, while forthcoming large-scale developments will open up areas that have been 'off-limits' for decades.

## Masterplan key:

### Accent wayfinding palette -

#### Creating identifiable connective stitches

The accent wayfinding palette outlines a set of bespoke wayfinding elements for key routes, the 'Stitches', within the Docks. User's of this design guide should refer to the accent wayfinding palette if their site is located on one of the following Stitches:

- Stitch 1: Canning Town to the Docks**  
Stitch wayfinding characterised by yellow colour theme continued from the Leaway.
- Stitch 2: Custom House to the Thames**  
Stitch wayfinding utilises low-key signage, heritage structures and orange-corten colour.
- Stitch 3: Connaught Crossing**  
Stitch wayfinding incorporates playful super-graphics to infrastructure and structures that move in the wind.
- Stitch 4: Beckton to North Woolwich**  
Stitch characterised by 'light touch' red colour coding of existing street furniture; handrails, signage, bridges.
- Stitch 5: Dock Loop**  
Stitch wayfinding along the dock edge incorporates bespoke signage, maritime artefacts and long views.

The dotted lines illustrate sections of the route currently inaccessible. The dashed lines illustrate secondary stitch routes. Locations on these routes can use the accent palettes.

### Baseline wayfinding palette -

#### A legible wayfinding strategy

The baseline wayfinding palette outlines a consistent approach to wayfinding across the Docks, incorporating the TfL range of Legible London signage. User's of this design guide should refer to the baseline wayfinding palette for information on implementing Legible London signage:

- Legible London totem - existing**  
Information points for navigation
- Legible London finger post - existing**  
Directional signage for navigation
- Legible London totem - planned**  
Information points for navigation
- Legible London finger post - planned**  
Directional signage for navigation
- Legible London totem - proposed**  
Information points for navigation
- Legible London finger post - proposed**  
Directional signage for navigation

### Accent wayfinding palette -

#### Bespoke elements

Range of proposals to be used on stitch routes. The locations of the accent wayfinding palette bespoke elements are indicative.

- Dock Loop threshold mat**  
Installed at key access point to the Dock Loop
- Dock Loop active mat / insert**  
Installed at approximately 100m intervals identifying the Dock Loop and supporting active uses
- Maritime objects**  
Installed regularly along the Dock Loop
- Area sign**  
Beacons installed at intersection of Dock Loop and the Stitches
- Public space beacons**  
Lattice columns installed at public spaces along Stitches
- High level elements**  
Additional large scale features along Stitches
- DLR viaduct piers**  
Treatment to aid in wayfinding along Stitches
- Stitch markers**  
Colour signs mounted on lighting columns along Stitch routes

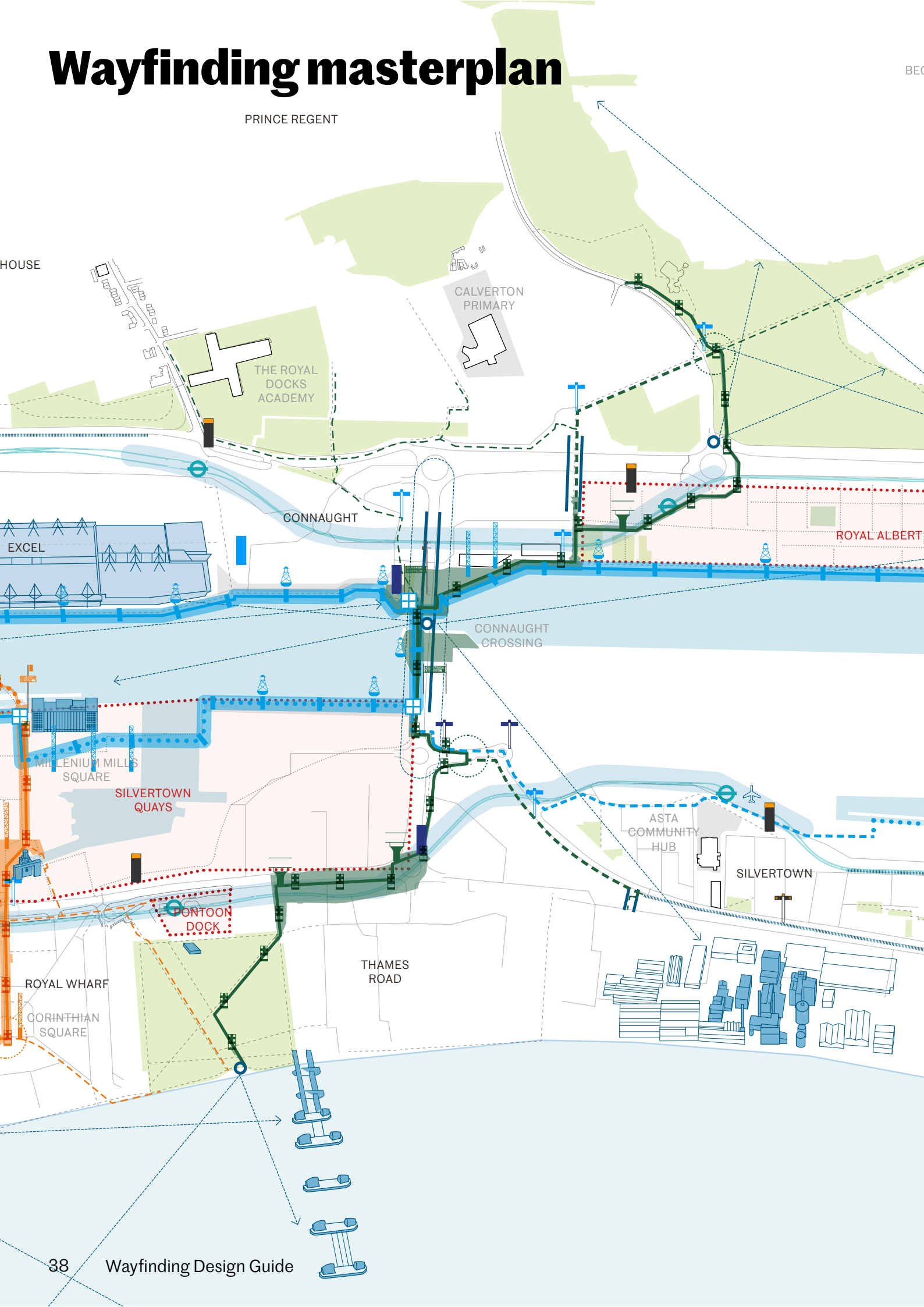
### Special treatments -

#### Places, landmarks and views

The special treatments palette recognises the important value of the Royal Docks' existing urban fabric and how it can play a key role in the wayfinding strategy. Users of this guide should incorporate existing built and landscape features into plans to improve the legibility and character of the area:

- View points**  
Long range vistas
- Landmarks**  
Prominent structures in the area
- Bridges**  
Key crossing points over the Docks
- DLR Viaduct**  
Piers supporting the DLR line
- Streetscape**  
Boundary walls, fencing, sidings
- Community facilities**  
Key focal points of community life

# Wayfinding masterplan



## Area description

The key north-south connection provided by the Connaught Crossing characterises this area, providing links to the historic marshland of Beckon Park, as well to existing communities around Prince Regent, Silvertown and Thames Barrier Park. The hulking presence of the Tate and Lyle refinery, as well as the airport and the views across vast expanses of dock water, give this area a super-scaled quality, reminiscent of the scale of operations that used to exist in the docks.

## Masterplan key:

### Accent wayfinding palette -

#### Creating identifiable connective stitches

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Stitch characterised by 'light touch' red colour coding of existing street furniture; handrails, signage, bridges.
- Stitch 5: Dock Loop**  
Stitch wayfinding along the dock edge incorporates bespoke signage, maritime artefacts and long views.

The dotted lines illustrate sections of the route currently inaccessible. The dashed lines illustrate secondary stitch routes. Locations on these routes can use the accent palettes.

### Baseline wayfinding palette -

#### A legible wayfinding strategy

The baseline wayfinding palette outlines a consistent approach to wayfinding across the Docks, incorporating the TfL range of Legible London signage. User's of this design guide should refer to the baseline wayfinding palette for information on implementing Legible London signage:

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Information points for navigation
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Directional signage for navigation
- Legible London totem - proposed**  
Information points for navigation
- Legible London finger post - proposed**  
Directional signage for navigation

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#### Bespoke elements

Range of proposals to be used on stitch routes. The locations of the accent wayfinding palette bespoke elements are indicative.

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- High level elements**  
Additional large scale features along Stitches
- DLR viaduct piers**  
Treatment to aid in wayfinding along Stitches
- Stitch markers**  
Colour signs mounted on lighting columns along Stitch routes

### Special treatments -

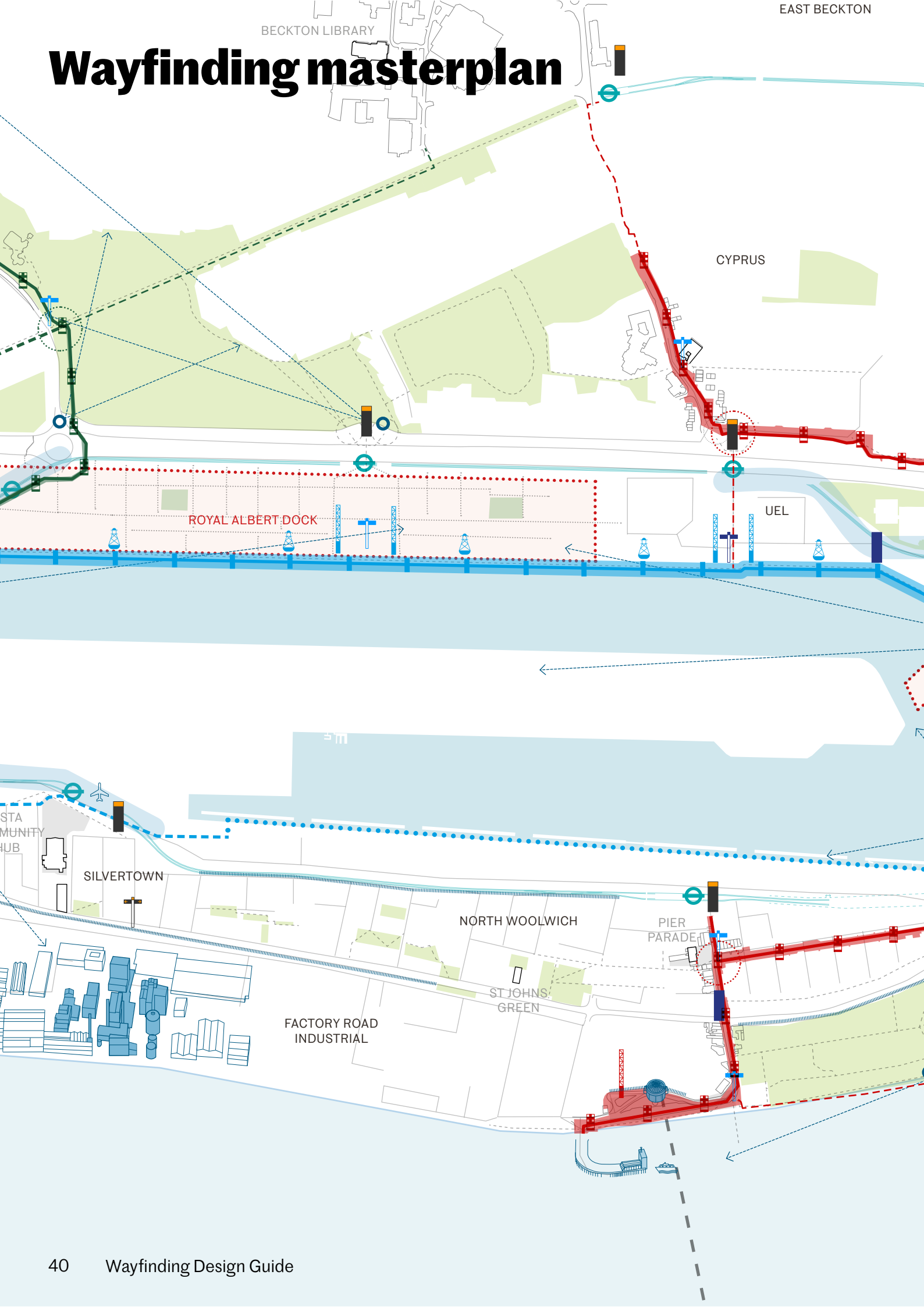
#### Places, landmarks and views

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Piers supporting the DLR line
- Streetscape**  
Boundary walls, fencing, sidings
- Community facilities**  
Key focal points of community life



# Wayfinding masterplan



## Area description

To the north and south of the dock water lie two very different conditions. To the north, the new development along Royal Albert Dock will establish a new urban condition on a currently vast and empty space – opening up the dock edge as well as connections to Beckton Park. To the south, the existing communities of Silvertown and North Woolwich lie between a working industrial Thames edge and the airport.

## Masterplan key:

### Accent wayfinding palette -

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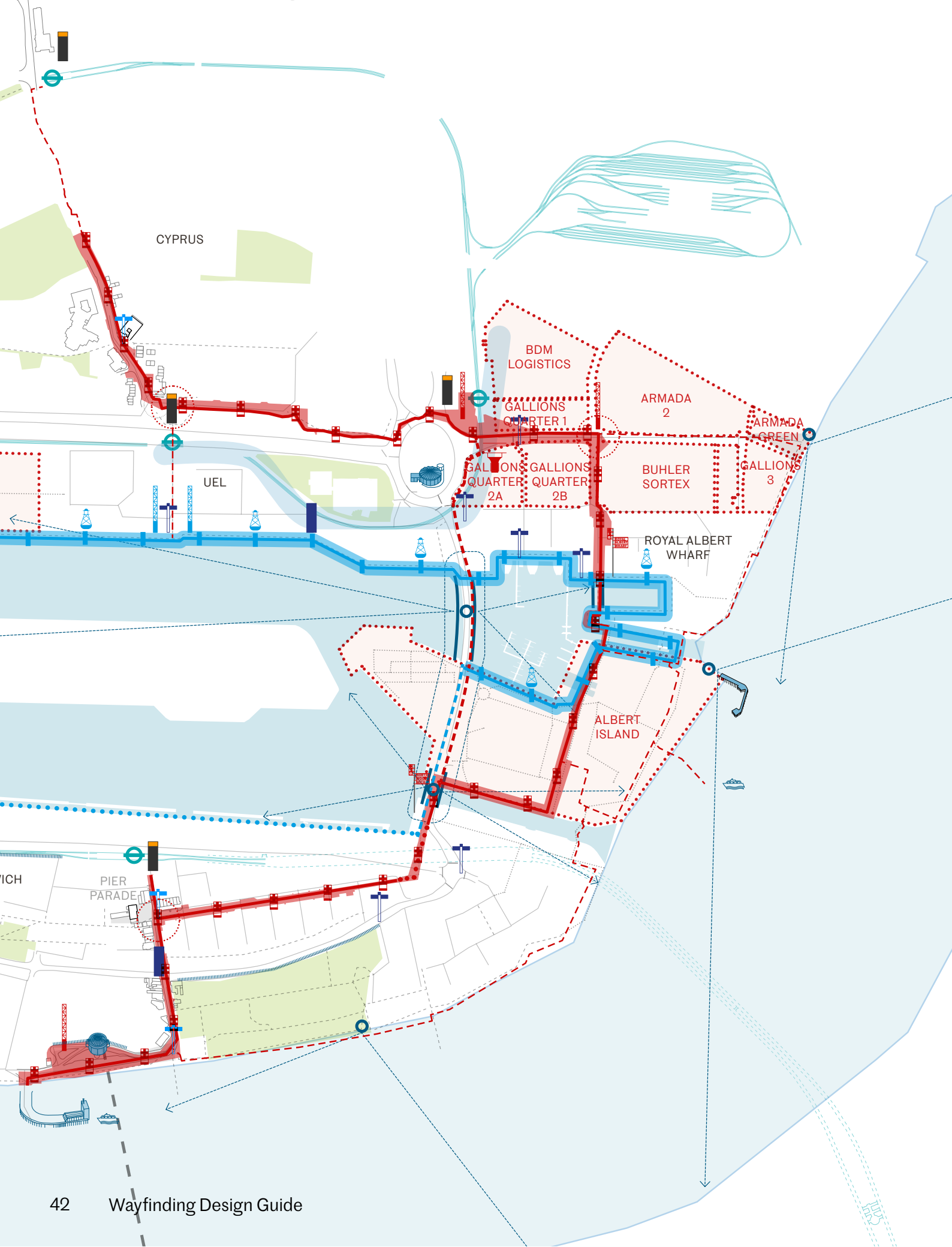
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*Piers supporting the DLR line*
- Streetscape  
*Boundary walls, fencing, sidings*
- Community facilities  
*Key focal points of community life*

# Wayfinding masterplan



## Area description

This area is characterised by its connection to the Thames, with the emerging Gallions communities to the north with their proximity to the dock edge, the planned development on Royal Albert island establishing a strong maritime presence on the Thames, and the historic area of North Woolwich, with its ferry, pier and pleasure gardens.

## Masterplan key:

**Accent wayfinding palette -**  
**Creating identifiable connective stitches**  
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Information points for navigation
- Legible London finger post - planned  
Directional signage for navigation
- Legible London totem - proposed  
Information points for navigation
- Legible London finger post - proposed  
Directional signage for navigation

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Long range vistas
- Landmarks  
Prominent structures in the area
- Bridges  
Key crossing points over the Docks
- DLR Viaduct  
Piers supporting the DLR line
- Streetscape  
Boundary walls, fencing, sidings
- Community facilities  
Key focal points of community life





The following chapter describes the wayfinding palette. This consists of a **site wide approach**, a **baseline palette** and an **accent palette**, alongside the guidelines for **special treatments** to existing assets.

A **site wide approach** ensures consistency and coherence of approach to signage across the Docks.

The **wayfinding baseline palette** outlines a set of 'best' practice approaches for directional signage.

The **wayfinding accent palette** sets out bespoke, characterful approaches to reinforce the Stitches.

**Special treatments** recognises the important existing structures and how these can be treated to aid in the wayfinding strategy.

The wayfinding palette has been developed to:

- Create a legible and coherent environment.
- Enhance the character and identity of the area.
- Establish a safe and inclusive means of navigation.

This chapter should be read in conjunction with the accompanying lighting, landscape and accessibility and inclusive design guides which make up the Royal Docks public realm designers' pack. Illustrative views of how the palettes inter-relate are illustrated in chapter 4, Worked examples.

# 3. Wayfinding Palette



# Palette structure and how to use

The wayfinding palette has been designed to work with the accompanying landscape and lighting palettes. This page illustrates the content of the palettes across the three public realm design guides, and how they should be used.

### Palettes structure

The table to the right shows the range of palette components across the wayfinding, landscape and lighting design guides, breaking them down into the four approaches; site wide, baseline, accent and special treatments.

### How to use

The stages below illustrate how users of this guide should access the palette sections, showing how the Masterplans (chapter 2), Palettes (chapter 3), Worked Examples (chapter 4) and Design Information (chapter 5) combine to provide a detailed approach to public realm improvements in the Royal Docks.

#### Masterplan

Use the masterplan to identify where your site is located in the Royal Docks. The key indicates which palette section to refer to. Detailed masterplans can be found in the appendices.

#### Palettes

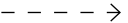
Read through the relevant palette sections to determine what public realm treatments should be applied to your site.

#### Worked examples

Refer to the worked examples, using the view with similar characteristics to your site, to see how the individual palette elements come together in the public realm.

#### Design Information

Find more detailed information on the public realm components outlined for your site based on the masterplans and palettes.



	Site wide approach	Baseline palette	Accent palette	Special treatments
Wayfinding	<ul style="list-style-type: none"><li>▪ Street signs</li><li>▪ Information boards</li><li>▪ Temporary signs for events</li></ul>	<ul style="list-style-type: none"><li>▪ Directional signage</li></ul>	<ul style="list-style-type: none"><li>▪ The Stitches</li><li>▪ The Dock Loop</li><li>▪ Stitch markers</li><li>▪ Area signs</li><li>▪ Public space beacons</li></ul>	<ul style="list-style-type: none"><li>▪ Landmarks</li><li>▪ Views</li><li>▪ Co-opting existing structures</li><li>▪ Public art</li></ul>
Landscape	<ul style="list-style-type: none"><li>▪ Hard surfacing</li><li>▪ Seating</li><li>▪ Street furniture</li><li>▪ Green Infrastructure</li><li>▪ 'Green Moves'</li><li>▪ Trees</li><li>▪ Planting</li><li>▪ Airport safeguarding</li></ul>	<ul style="list-style-type: none"><li>▪ Hard surfacing</li><li>▪ Play</li><li>▪ Trees</li><li>▪ Planting</li></ul>	<ul style="list-style-type: none"><li>▪ The Stitches</li><li>▪ Trees</li><li>▪ Planting</li><li>▪ Hard surfacing</li><li>▪ Street furniture</li></ul>	<ul style="list-style-type: none"><li>▪ Water's edge boundary</li><li>▪ Planting in the basins</li><li>▪ Exposure</li><li>▪ Shelter</li><li>▪ Wildness</li><li>▪ Playfulness</li><li>▪ Cultivation</li><li>▪ Colonisation</li></ul>
Lighting		<ul style="list-style-type: none"><li>▪ Public routes</li><li>▪ Local roads</li><li>▪ Major roads</li></ul>	<ul style="list-style-type: none"><li>▪ The Stitches</li><li>▪ The dock edge</li><li>▪ Dock edge objects</li><li>▪ Coordination with wayfinding palette</li></ul>	<ul style="list-style-type: none"><li>▪ Diverse places</li><li>▪ Buildings and structures</li><li>▪ Public spaces</li><li>▪ Darkness</li></ul>



# Site wide approach

The Royal Docks contains a multitude of signage, a reflection of the area's piecemeal development without a unifying approach. Individual estates have adopted various different styles of displaying information in contrast to one another, an approach that has left the docks with a museum of signage.

The 'site wide approach' sets out principles and guidelines for signage that is to be used in all areas within the Royal Docks. This approach aims to introduce consistency across the area and reduce the amount of obsolete and obstructive signage to provide a coherent, legible public realm that looks and feels connected.



London Borough of Newham standard post mounted street sign

## Standard signage

Standard signage within the Docks should, where appropriate, comply with the London Borough of Newham's standards. Highways and public realm signage should also be compliant with the TfL Streetscape Guidance 2019 (<http://content.tfl.gov.uk/streetscape-guidance-.pdf>).



Visual clutter from multiple style of signage

## Minimising visual clutter

Visual clutter caused by multiple signs in close proximity should be avoided. The number and variety of signs should be considered in conjunction with the character and function of the public realm, as well as the placement of other street furniture.





# Standard signage

## Street signs

London Borough of Newham standard street signage should be adopted on both public and private land to maintain continuity and counteract estate isolation. Street name plates can either be set on double posts (900mm high) or a single post (2500mm high) to suit location. Wall mounted plates are to be fixed to the structure with suitable plugs and galvanised screws, preferably set at a height of 2.5m, but not more than 3m above the road level.

To install street signs on private land, an application must be made to the London Borough of Newham Council for street names and postal numbers for new or converted properties. Locations are to be in accordance with the approved drawings or as agreed on site by the London Borough of Newham's representative. The developer is responsible for all costs if the sign is installed on private property. If the road is adopted by the London Borough of Newham then the local council will become responsible for any maintenance costs.

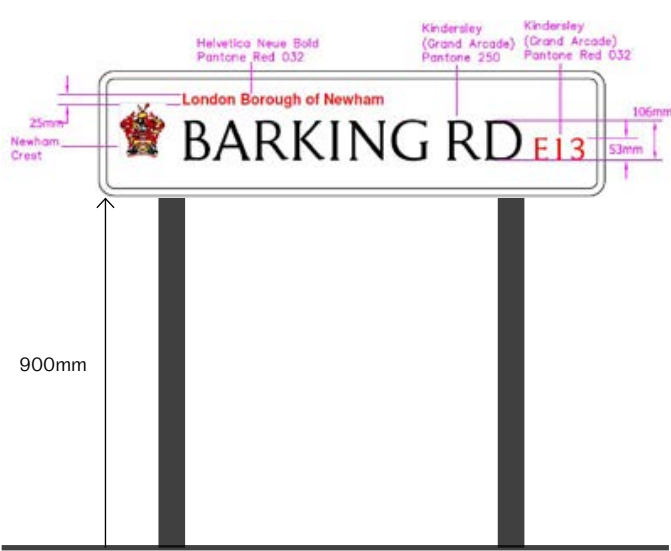
## Post mounted signs

In certain situations, post-mounted signs are necessary for informing the public of land rules and regulations as well as safety instructions. The amount and positioning of signage should be carefully considered, and the number of posts. Particular effort should be made to avoid having too many of the same signs close to one another, or overload sites with different types of signs.

The installation of post mounted signs should be in accordance with the following criteria:

- Affixed to round posts or existing lighting posts not obstruct footway movement or visibility be of a neutral, compact design.

Further guidance on the use of post mounted signs can be found in the TfL Streetscape Guidance 2019 (<http://content.tfl.gov.uk/streetscape-guidance-.pdf>).



London Borough of Newham double post mounted street sign technical specification



Too many signs cause clutter. Signs should be combined where possible.

# Information boards

## Community Infrastructure

The Royal Docks is home to a diverse population with many active community groups working to bring residents together. The public realm however is poorly equipped to support community uses, with poor infrastructure to facilitate the activation and stewardship of local spaces.

Information boards can support community organisation by informing people of the activities taking place in their area. Developers, the local authority and project commissioners should set briefs for designers to develop proposals for community based information boards on a site by site basis. Community groups should be involved in the co-production and ownership of these boards to ensure that the infrastructure and access provided responds to local needs.

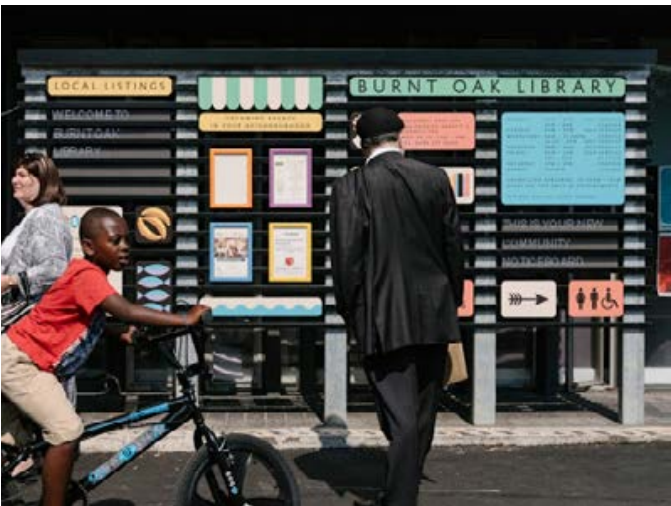
Information boards should be installed in residential neighbourhoods such as Britannia Village and North Woolwich, preferably at meeting spots such as local parks, squares and outside key community facilities. Fixed board information should comply with Royal National Institute of Blind People (RNIB) guidelines for viewing distances and font size / style and be lit at night for increased usability (refer to the Lighting Design Guide).

Boards should be mounted on 'Traffic Grey' posts to match other street furniture. If located on a stitch, colour could be incorporated into the design (refer to accent palette, chapter 4, for stitch colours). Boards should be constructed out of robust, weather resistant materials, and require little ongoing maintenance.

## Historic Information

The Royal Docks has a rich history which has played a crucial role in its development and the lives of the people who live there. Boards detailing specific historic moments, structures, sites and figures would welcome residents and visitors alike to engage with the Royal Docks' past, providing an educational experience. In this manner, historic information boards can act as a creative tool for storytelling and promote a narrative-based understanding of the area.

Historic information boards should be of a neutral, robust design, post-mounted at a comfortable reading level and be positioned so as not to obstruct movement and visibility. Materials used should be hard-wearing and require little ongoing maintenance and renewal, short of damage.



Information board, Burnt Oak Library, London

**RAL 7043**  
**Traffic Grey**

Standard street furniture RAL colour for board mounting structure

## Innovation

Engaging local communities through monitoring air quality and providing visual data in information boards and signs.



Example of a proposed historic information board at Connaught Crossing



# Temporary signage for events

The Royal Docks will be host to numerous events ranging from small scale community activities to larger cultural and commercial programmes. Temporary events can activate the public realm and ensure the community can benefit from positive meanwhile uses of undeveloped sites. To support event promotion and the on-day experience, a considered approach to the quality, amount and positioning of temporary event signage is necessary to avoid causing undue impact on the public realm.

The event signage palette illustrated here should be used for all events in the Royal Docks. The criteria, objectives and planning process for temporary events can be obtained by contacting the Royal Docks Team. For information on lighting supporting temporary events, refer to the Lighting Design Guide.

**Integrated banner and lighting column**  
Banners advertising events can contribute to the sense of an active and vibrant public realm, and act as visual stimuli to aid in navigating the area. Banners may be affixed to existing lighting columns to reduce the need for additional posts providing that:

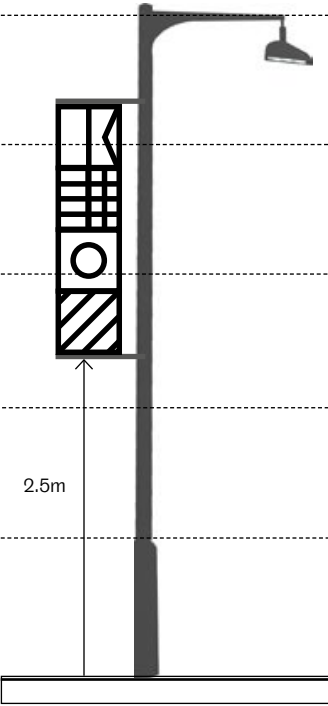
- The lighting column can take the load of the banner.
- The banners and fixings are removed within an agreed time frame and with no damage caused to the permanent street furniture.
- The banner and fixings do not cause road safety or visibility concerns.
- The banners do not compromise the lighting environment.

Banners should be positioned at least 2.5m above ground for safety and to prevent damage. Banners could be installed either as a single or double unit, depending on the size and position of the lighting column.

Event organisers wishing to install banners on existing lighting columns will need to consult with the London Borough of Newham lighting officer to determine appropriate locations subsequent to structural surveys being carried out.

Temporary event signage should be designed and maintained in accordance with the following criteria:

- **Formatting** - Signs should be clear and communicative for all, using where appropriate the Royal Docks branding identity. Signs should be appropriately sized according to the information and mounting system.
- **Mounting** - Sign mounting systems should be easily accessible to install and remove signage. While installed, signs should be safely secured and not require ongoing maintenance.
- **Location** - Signs should be positioned to not overly impinge on footways and visibility. Public safety should be a primary consideration when locating signs.
- **Maintenance and Removal** - Signs should require little to no maintenance for the duration of their use. The event organiser is responsible for removing all signage after an event.

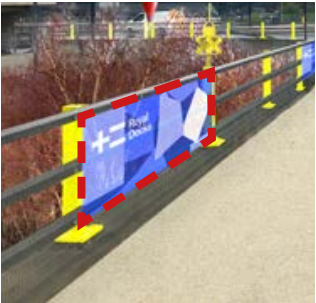


Signs fixed to balustrades are strongly recommended to be avoided if possible - this is to reduce the amount of visual clutter in the public realm and minimise the risk of signage not being removed after the event.



Event signage fixed to lighting post

- Illustrative design for banner mounted on lighting post



## Free-standing signs

Free-standing frame-mounted signs can be used by organisers for event promotion and navigation on temporary sites. Signs should be present and readable, and include event posters and directional information generally required such as 'toilets', 'fire exits', 'first aid', etc.

Sign structures should consist of a simple frame design, with event information elevated more than 1m above ground and a lower panel to conceal frame counter weights, also acting as an alert for cane users. The frame structure could be designed to stand alone or as part of a modular assembly and be able to stand independently of the surroundings. Further guidance on the design of inclusive signage can be found in the 'Sign Design Guide', 2000, published jointly by the Sign Design Society and the Royal National Institute of Blind People.

## Gateway structures

The use of gateway structures can clearly signal the location of temporary sites and provide a celebratory landmark for the duration of the event. Event organisers and signage designers should identify appropriate locations at key access points to ensure they have the most visual impact.

Where appropriate, gateway structures should work with the existing public realm to creatively enhance pedestrian and cycle routes. In no way should the structure negatively impinge on the public realm by obstructing important movement and visual corridors.

## Mounted signage

Using existing structures can be a more cost effective solution to providing large scale signage at event sites. Temporary signs may be mounted to existing or new structures. Mounted signs must be appropriately scaled to suit their location and the information presented readable to all.

For existing structures, such as buildings or walls, the structure must be accessible and be able to take the load of the signage. Permission must be sought from the owner and be installed and taken down in the agreed time frame.

For new structures with mounted signage, the overall design must suit the location so as not to obstruct or hinder the public realm.



Free standing signs with posters, Strelka Institute



Gateway structure, La Cantine Du Voyage,



Mounted sign, La Machine company

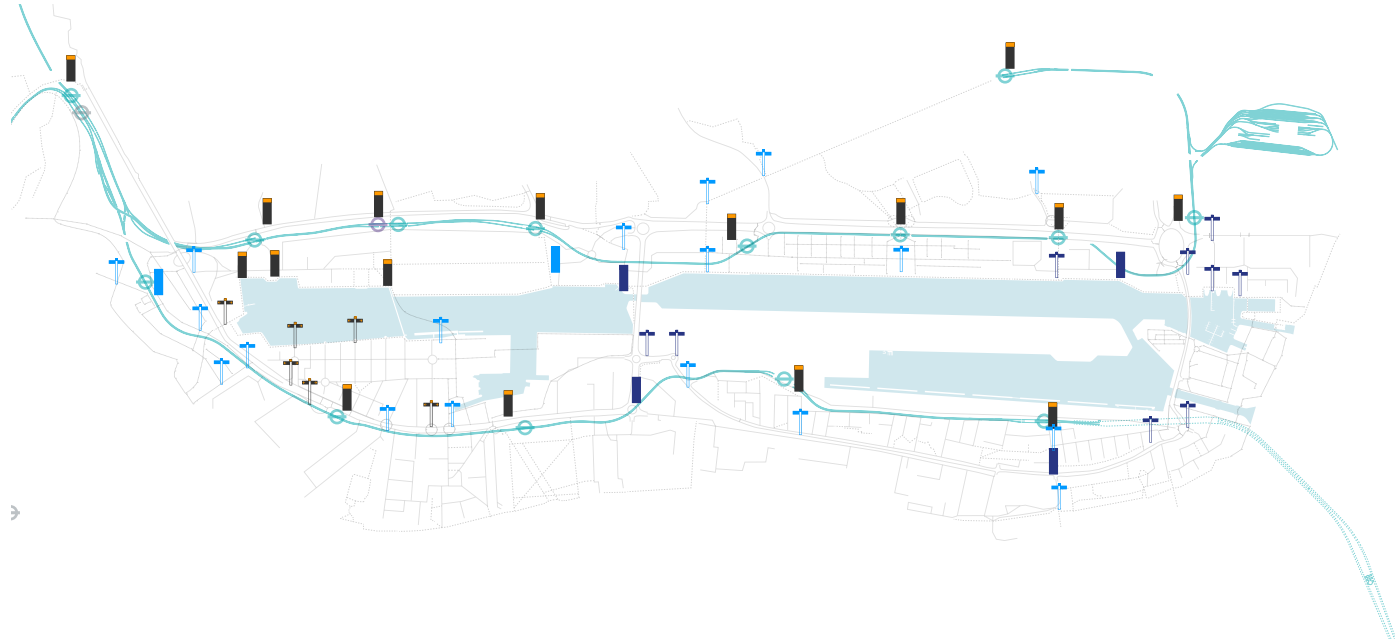


# Baseline palette

The wayfinding 'baseline palette' supports the continuing roll out of Legible London signage throughout London's Boroughs. The TfL range of signage provides a coherent and familiar approach to wayfinding across the Royal Docks and the wider context.

The scheme illustrated in the wayfinding baseline masterplan identifies the locations for new Legible London signage, as well as existing and planned Legible London projects. The strategy for locating Legible London signage has been carefully considered to reflect the needs of ongoing development of the area, and has been reviewed by the Royal Docks Team and the London Borough of Newham.

Wayfinding baseline masterplan





# Directional signage

## Legible London

All new development sites should adopt the standard TfL Legible London proprietary signage, already installed in several locations within the site and the wider London Borough of Newham, as the primary method for displaying directional information for wayfinding in the Royal Docks.

Signage should be clearly located, placed at key decision points along important connections in all areas of the Docks. Within the Legible London range, finger posts are used to connect residential neighbourhoods, streets and transport links. At key sites - such as DLR station exits - Legible London Midiliths should be used to inform users of orientation, route distances and times, adjacent attractions, and transport connections.

The scheme coordinator is responsible for raising the funding for and implementing a new Legible London scheme through a Delivery Agreement with TfL. Legible London signage is manufactured and supplied through a prestanding contract with Trueform. Local authorities can use the TfL framework to access this call-off contract with Trueform, simplifying the procurement process. To arrange this, contact Legible London at TfL.

The authority or organisation that funds and installs the Legible London signs in an area is also responsible for maintaining them. TfL maintains any Legible London signs on TfL's red routes (<http://lruc.content.tfl.gov.uk/red-route-pan-london-lohac-map.pdf>), and also maintain and update the Legible London base map. The sign map panels are pulled from a pan-London mapping database.

## Innovation

Working with the local community to develop a tactile map to supplement Legible London signage to show the area plan and key sites.

## Legislation

Traffic Signs Regulations and General Directions (TSRGD)  
Transport for London:  
Legible London Design Standards, 2010  
Legible London: <https://tfl.gov.uk/info-for/boroughs/legible-london>

## Innovation

Data collection in the public realm, from sensors, digital street furniture, advertising and other new technologies being embedded into street furniture.



Example of unnecessary duplication of directional signage cluttering the public realm



Legible London fingerpost proposed at the dock edge near Britannia Village



Legible London midilith proposed at Connaught Crossing



Legible London range of signage



Legible London fingerpost installed in the Royal Docks



Legible London midilith installed in the Royal Docks



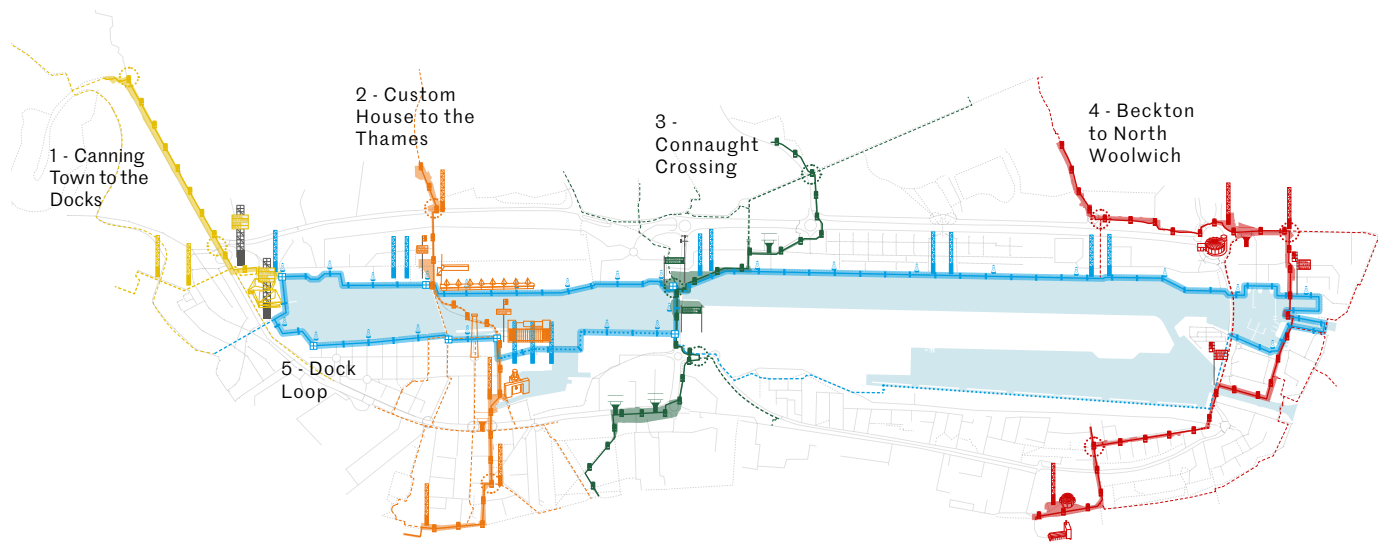
# Accent Palette

The wayfinding 'accent palette' sets out approaches for the Stitches, which connect key places and public spaces across the docks. The Stitches have an important role in establishing coherent routes at the few places where you can cross north-south and access the dock edge.

The Stitches also help to strengthen broader strategic connections between the London Borough of Newham's residential neighbourhoods and the Royal Docks, and to tie together the diverse social, physical and historical context of the area.

The Stitches support wayfinding through establishing better, more coherent and legible places. The accent palette provides a range of bespoke wayfinding elements that work alongside the approaches set out in the Lighting and Landscape Design Guides.

Accent palette masterplan





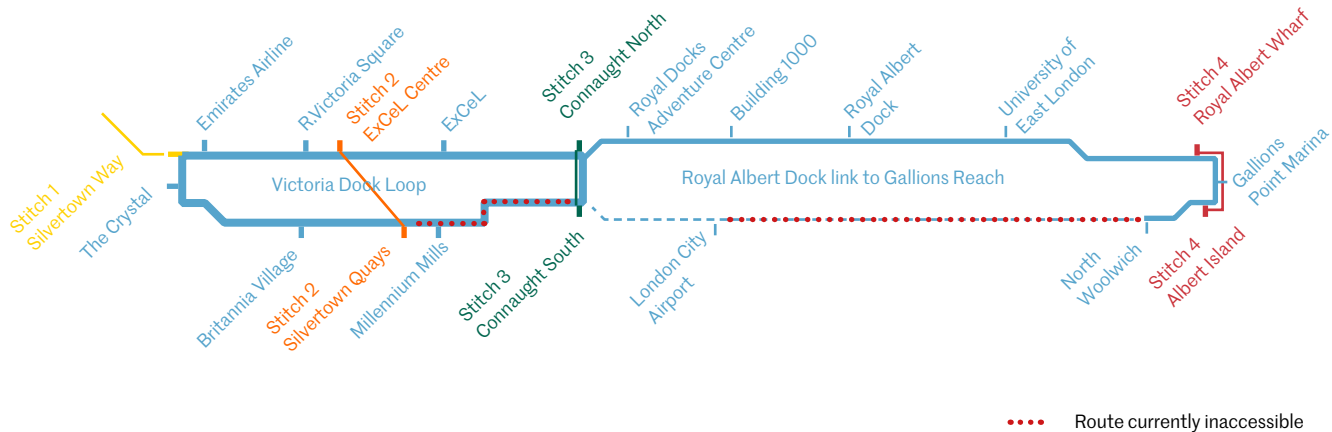
# The Stitches

The Stitches cross the Royal Docks in important locations. They each:

- Reach or bridge the dock water.
- Link between DLR stations.
- Link to strategic connections in Newham.
- Integrate existing infrastructure.

Each stitch has its own character, and also varies considerably along its route. Unwrapping the stitches reveals the numerous landmarks and places linked by the route. The wayfinding strategy connects these through the tactics illustrated in the following pages.

## Route features



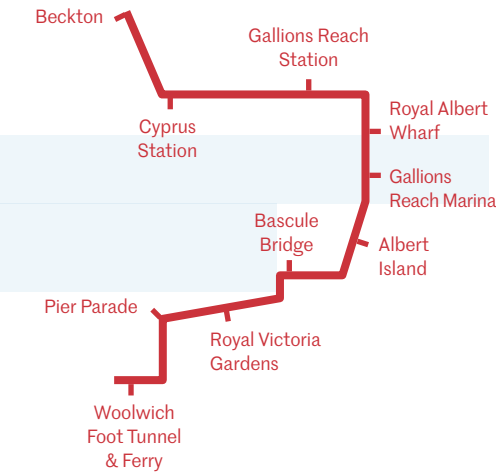
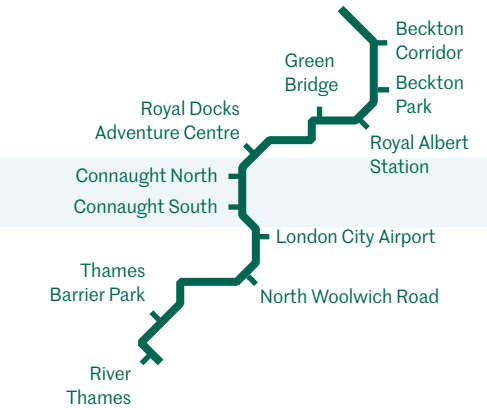
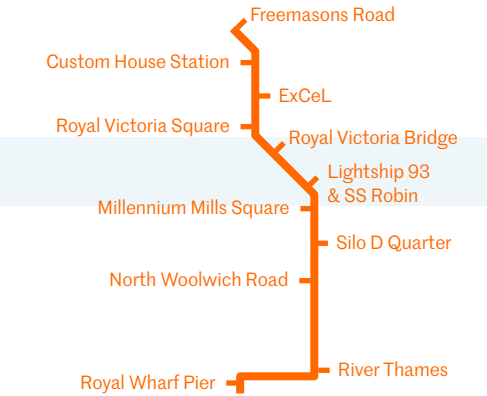
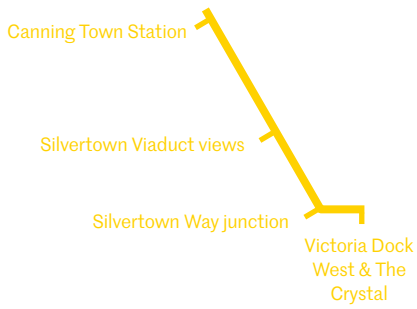
**Canning Town to the Docks**  
Connecting the Leaway to the Royal Docks, via Canning Town, creates a better connection between two of Newham's major growth areas, establishing the link from the Queen Elizabeth Olympic Park and Canary Wharf to the Royal Docks. In this manner, this stitch acts as an important gateway to the area.

**Custom House to the Thames**  
Centuries of infrastructural works at the edges of the docks have created significant barriers to movement between the communities around Freemasons Road, the docks, and the River Thames. Overcoming these severances will lead to a diverse series of connected spaces and greater access to cultural and recreational activities around the docks.

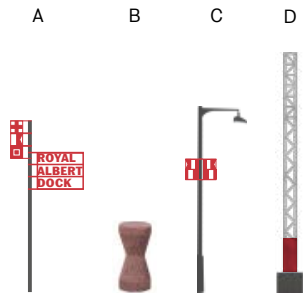
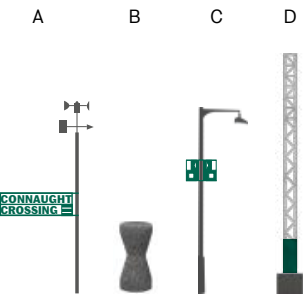
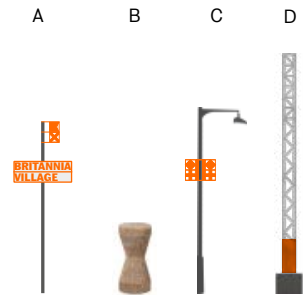
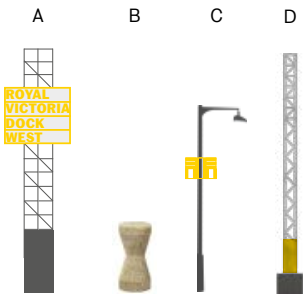
**Connaught Crossing**  
An improved offer of leisure and recreational activities, as well as connectivity improvements which seek to humanise the scale of existing road and DLR infrastructure, can reconfigure the Connaught Crossing as an 'armature for local amenities' and a destination in its own right.

**Beckton to North Woolwich**  
Poor quality infrastructural environments such as the Steve Redgrave Bridge, Woolwich Manor Way and Factory Road hinder a strong sense of place, north and south of the docks. Local connections and public realm improvements could ensure that these two established communities have a renewed connection to the Thames and its maritime heritage.

## Route features



## Bespoke wayfinding elements



# The Dock Loop

The dock edge is currently a well-used public space, providing long expanses of waterside routes for pedestrians, cyclists and runners. The Dock Loop stitch incorporating the proposed continuous 'Dock Loop' around Royal Victoria Dock and the connection to Gallions Reach along Royal Albert Dock will provide even greater access to the water's edge and provides the opportunity for the installation of new public realm elements.

The Dock Loop wayfinding elements promote a calmer, consistent approach to establish the stitch identity, recognising that the dock water itself acts as the primary wayfinding tool marking out the route. This includes lighting columns of the same design, perches and water fountains integrated with ground elements at regular intervals and the use of curated artefacts related to the quayside operations and the Docks' history to enhance the maritime theme. The following pages outline these elements in more detail and where they should be used along the Dock Loop stitch.



Ground paving signs, Leaway River Park, London

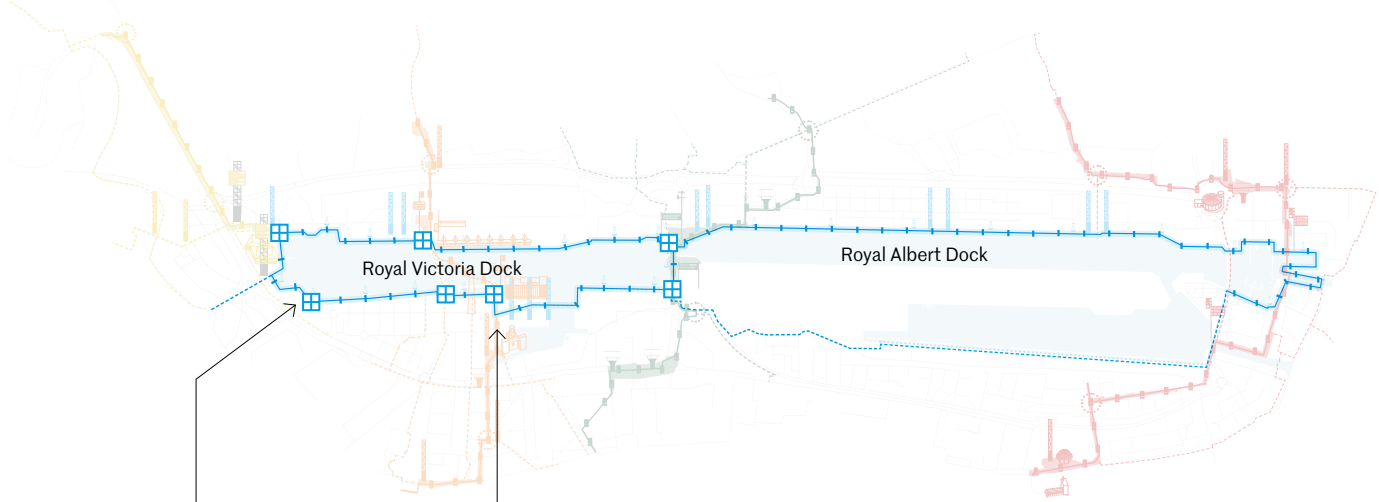


Consistent lighting columns



Maritime buoys

## Dock Loop plan



Threshold mats at intersections with stitch and key entrances to the Dock Loop

Important to provide mats at decision points away from the water to ensure users stay on the Dock Loop

Key

Threshold mat

Route markers/Active mat

### Dock Loop mats

Acting as waymarkers and sites of street furniture along the dock edge, the proposed mats will also provide coherence and legibility. They will integrate into the existing concrete pavers and granite setts, but can also be combined with the proposed hard surfacing for areas of new or re-paved dock edge.

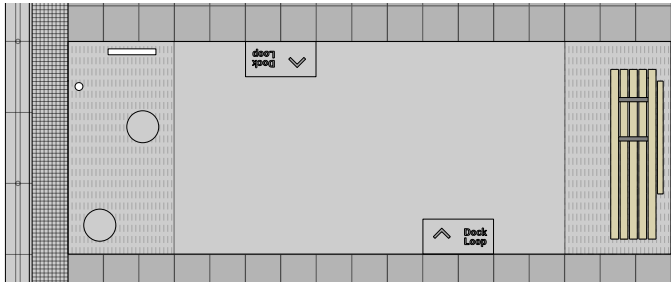
There are three options for installing mats along the Dock Loop; Entrance / Threshold mats, Active mats, and inset route markers. User's of this guide should refer to the landscape masterplan, chapter 2, and the plan opposite to establish which mat is appropriate for their site. For further guidance on the Dock Loop hard surfacing palette, refer to the Landscape Design Guide.



Threshold paving mat with dock loop sign and distance markings installed at Connaught Crossing

### Entrance/Threshold mat

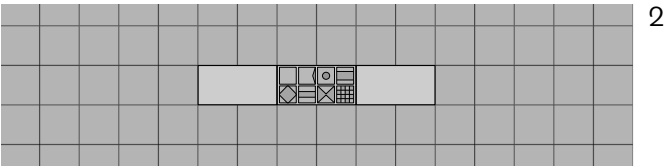
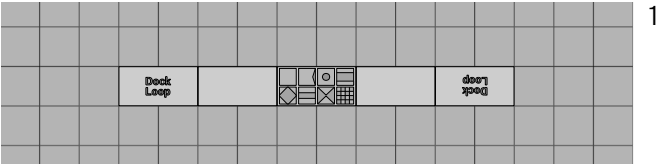
Large concrete paving mats incorporating street furniture and directional paving slabs. Positioned at key thresholds to the Dock Loop as illustrated in the plan opposite, where it intersects with the Stitches or prominent entrances to neighbouring communities.



### Inset route markers

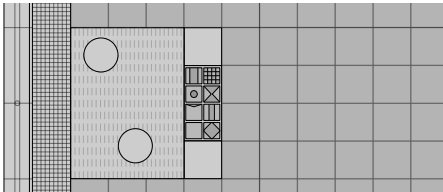
These are a collection of bespoke cast paving slabs set within the existing paving slabs. Positioned at regular intervals along the route to assist with wayfinding and to give character.

Option 1 is to be used on the Royal Victoria Dock Loop only, illustrated in the plan opposite. Option 2 can be used along all parts of the Dock Loop Stitch connecting Royal Victoria Dock West to Gallions Reach.



### Active mat

Brushed concrete mats, with identity slabs, to master street furniture - ensuring that the public realm is ordered and doesn't feel cluttered. Positioned periodically along the route where street furniture is to be placed near the dock water.



Active mats should be used in all sections of the Dock Loop stitch to organise street furniture at the waterside.



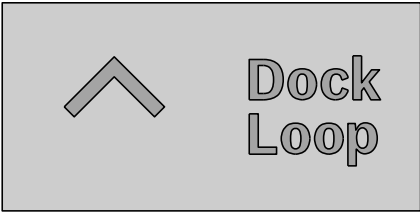
# The Dock Loop

### Dock Loop pavers

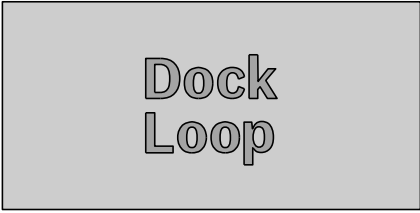
The Dock Loop should be clearly signed with markers on the ground (instead of on posts) so as not to obstruct the footway. The dock loop sign should be installed at approximately 100 metre intervals along the Dock Loop route as part of larger paving slabs that incorporates distance markings to support fitness activities along the waterside.

The Dock Loop sign is to be a pre cast concrete slab measuring 885mm x 440mm with inlaid text and graphics and designed to work independently of other street furniture and integrate with the existing block pavers most commonly found on the dock edge. The drawings here show the three types of Dock Loop signage to be used along the route.

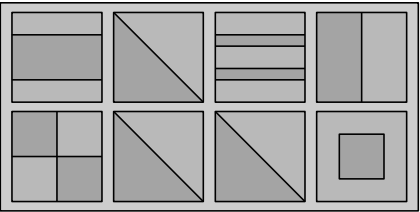
Refer to chapter 5, Design Information, for further details on the specification of the dock loop sign.



**Directional**  
To be used along the Royal Victoria Dock Loop where the route is not apparent and further direction is required



**Route**  
To be used periodically along the Royal Victoria Dock Loop route to give identity



**Identity and access**  
To be used periodically along the entire Dock Loop stitch to give identity, as well as to give accents to active mats



Dock Loop lighting columns and active mat with identity paver and perches. View along Royal Albert Dock.

### Dock Loop perches

For information on the perches and other street furniture to be used on the Dock Loop stitch, refer to the Landscape Design Guide.

### Dock Loop lighting columns

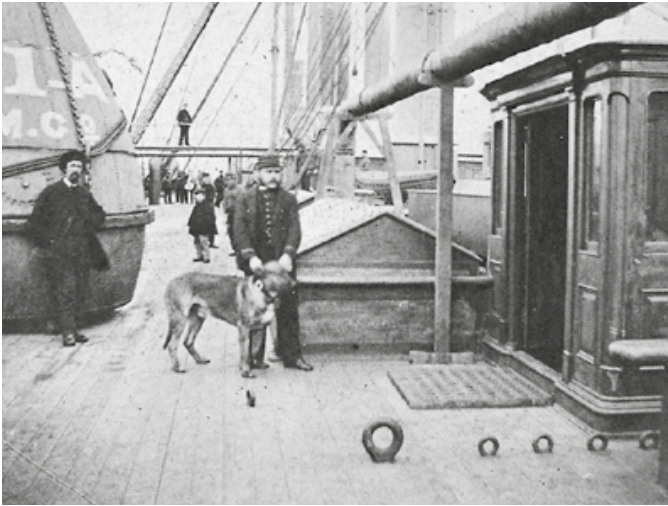
For information on the lighting columns used along the Dock Loop stitch, refer to the Lighting Design Guide.

### Maritime Artefacts

To enhance the nautical character of the Dock Loop, existing maritime artefacts should be retained and incorporated into any public realm improvements. On activated stretches of the dock edge, maritime objects could be reclaimed to aid in wayfinding. User's of this guide looking to install maritime objects on the dock edge should contact the Docklands Museum or the London Museum of Transport for an inventory of objects to be used.

Maritime objects should be located off the footway and integrated with planting where possible. For objects sitting on hard surface, it is preferable that these have tactile floor finish around them if they have a substantial lean or incline or are lower than 1000 millimetres.

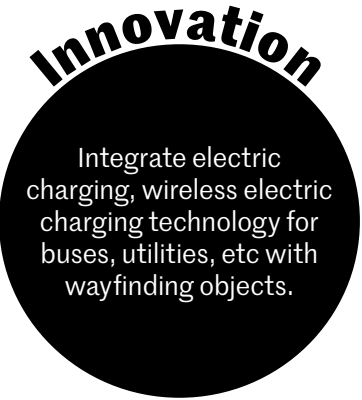
Maritime objects should be considered in conjunction with other street furniture and be positioned appropriately. Objects could be placed individually or as a family depending on the size. Maritime objects should not be used where they will interfere with movement or hinder visibility. The strategy for maritime objects should be done in close collaboration with the Royal Docks Management Authority (RoDMA).



Reference the Docks' history



Existing mooring posts and capstans at Royal Albert Wharf



Reclaimed marine buoy supporting stitch identity near Britannia Village



Reclaimed anchor located at Connaught Crossing



# Identifying the Stitches

In order to define the particular character of each stitch a number of complimentary approaches should be adopted - applied colour, materials, lighting and signature trees. Refer to the landscape and lighting design guides for additional accent palettes / stitch treatments.

**1 - Canning Town to the Docks**  
Stitch 1 continues the Leaway colour code to visually connect the Royal Docks and Canning Town. The colour specification references the colour highlighted at Canning Town station and Twelve Trees Crescent, and will stand out against the backdrop of the Silvertown Way highway.

**A) Applied colour:**  
RAL powder coating to bespoke wayfinding signage.



Canning Town Station, London

**B) Materials:**  
The combination of robust, self finished materials like galvanised steel against applied colour to signage.



Twelve Trees bridge, London

**C) Lighting:**  
Improvement of the pedestrian environment with additional low level lighting to footways.



Warm temperature lighting

**D) Signature trees:**  
Aspens as marker trees along the highway creating a boulevard character to the route.



Marker Aspens

**2 - Custom House to the Thames**  
Stitch 2 is to be characterised by weathered materials such as Corten steel that references Royal Wharf Pier and the rich maritime industrial history of the docks. The colour specification for signage reflects these materials.

**A) Applied colour:**  
RAL powder coating to bespoke wayfinding signage.



Royal Wharf Pier, London

**B) Materials:**  
Weathered materials like Corten steel for tree planters and brick dust added to concrete aggregates for perches.



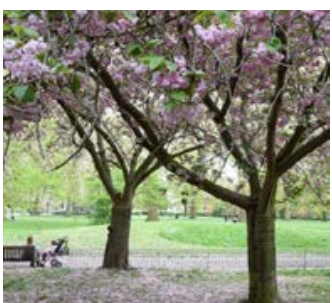
Weathered steel sign

**C) Lighting:**  
Warm temperature lighting to public spaces in softer concentrations through washes of light.



Light colour washes

**D) Signature trees:**  
Fragrant cherry trees forming part of the edible and sensory landscape approach.



Fruiting Cherries

**3 - Connaught Crossing**  
Stitch 3 is identified by the 'green link' between Beckton Park and Thames Barrier Park. The colour specification supports this link through the concrete landscape of the crossing and North Woolwich Road.

**A) Applied colour:**  
RAL powder coating to bespoke wayfinding signage.



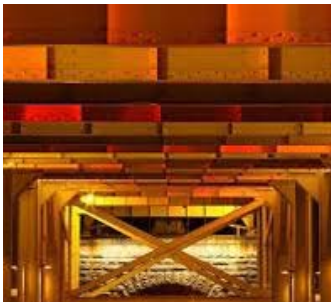
Painted columns, the Greenway

**B) Materials:**  
Emphasis on the linear landscape linking the Beckton Park and Thames Barrier Park; green planting a consistent feature of the route.



Green Bridge on Royal Albert Way

**C) Lighting:**  
Feature lighting to infrastructure such as Connaught Bridge underpass and the DLR viaduct on North Woolwich Road.



Infrastructural lighting

**D) Signature trees:**  
Pollarded willows reflecting the exposed, wild nature of the crossing.



Pollarded Willows

**4 - Beckton to North Woolwich**  
Stitch 4 highlights the historic drover's route along Woolwich Manor Way connecting Woolwich foot tunnel and ferry to Beckton. The colour specification is inspired by the red brick dock buildings found along the route and the area's maritime character.

**A) Applied colour:**  
RAL powder coating to bespoke wayfinding signage.



Painted railings, The Wirral, Liverpool

**B) Materials:**  
Materials that pick up on the red brick found in multiple quayside buildings found along the route.



Woolwich Foot Tunnel building

**C) Lighting:**  
Pools of light and colour to public spaces. Focussed and more nuanced lighting to preserve areas of darkness for biodiversity.



Lighting pools

**D) Signature trees:**  
Pine groupings that provide shelter and fit with the thameside character of the route.



Pine groupings



# Stitch markers

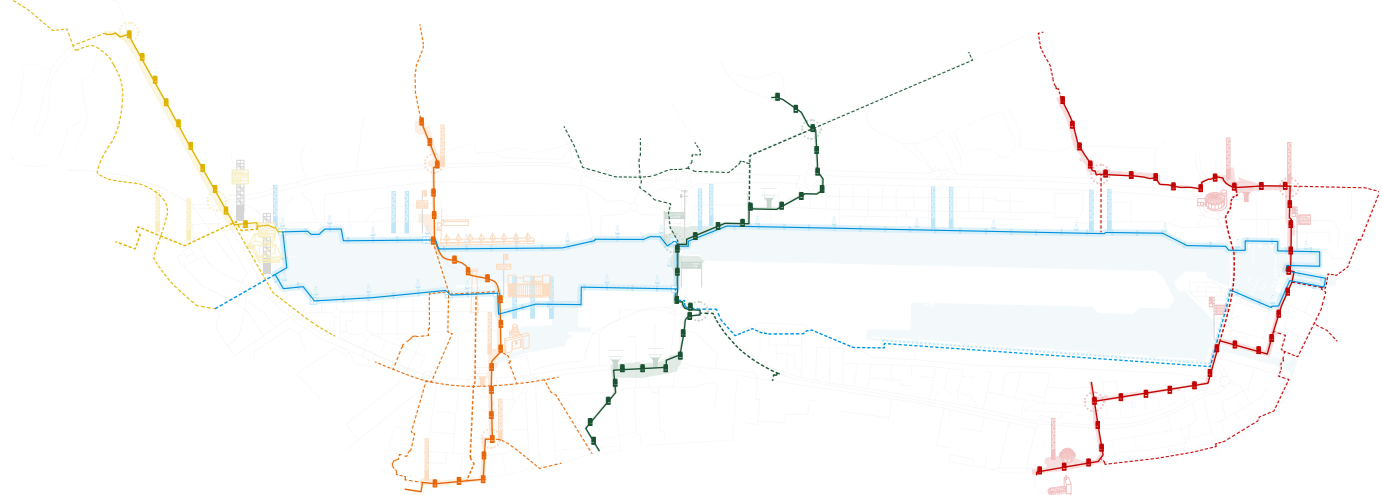
As the Stitches vary considerably along their route, picking up local neighbourhoods, commercial centres, public parks, highways, etc and covering several land ownership boundaries, it will take time to establish coherent places through landscaping and lighting improvements. As certain sections of each stitch will be developed before other parts, a series of 'Stitch markers' that can be fixed to existing lighting columns will help identify the Stitches before the other tactics can be fully rolled out in the public realm.

The markers establish the stitch route using the applied stitch colour and use symbols taken from the maritime flags, building upon the established use of these symbols across the area to create a recognisable and distinct identity for the Royal Docks. To simplify the language, Stitch markers should refer to one of three locations - areas north of the dock water, the Royal Docks and the River Thames. For directions towards the Royal Docks for example, the markers take the initials R and D to create the plus and minus symbols.

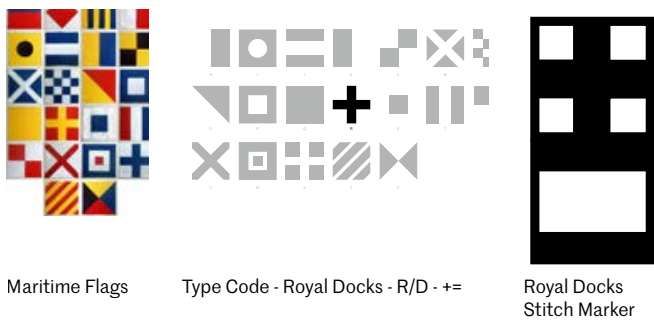
Markers should be mounted on lighting columns in clearly visible locations at approximately 100m or less intervals and are to be mounted at least 3m above ground level to prevent obstructing sight lines and deter vandalism. The London Borough of Newham will need to approve the use of lighting columns prior to installation.

For further details on the specification and maintenance of Stitch markers, refer to chapter 5, Design Information.

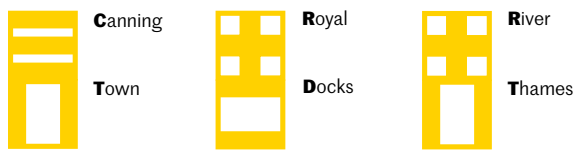
## Stitch marker plan



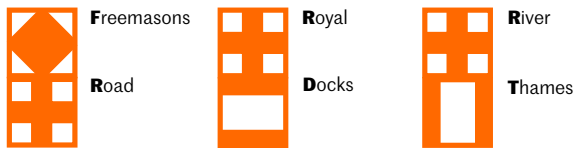
Stitch markers to feature prominently on route, located at approximately 50-100 metre intervals.



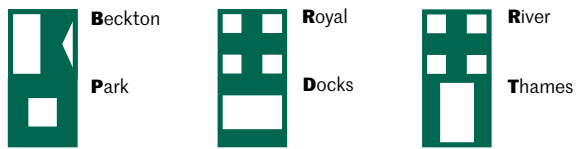
### 1 - Canning Town to the Docks



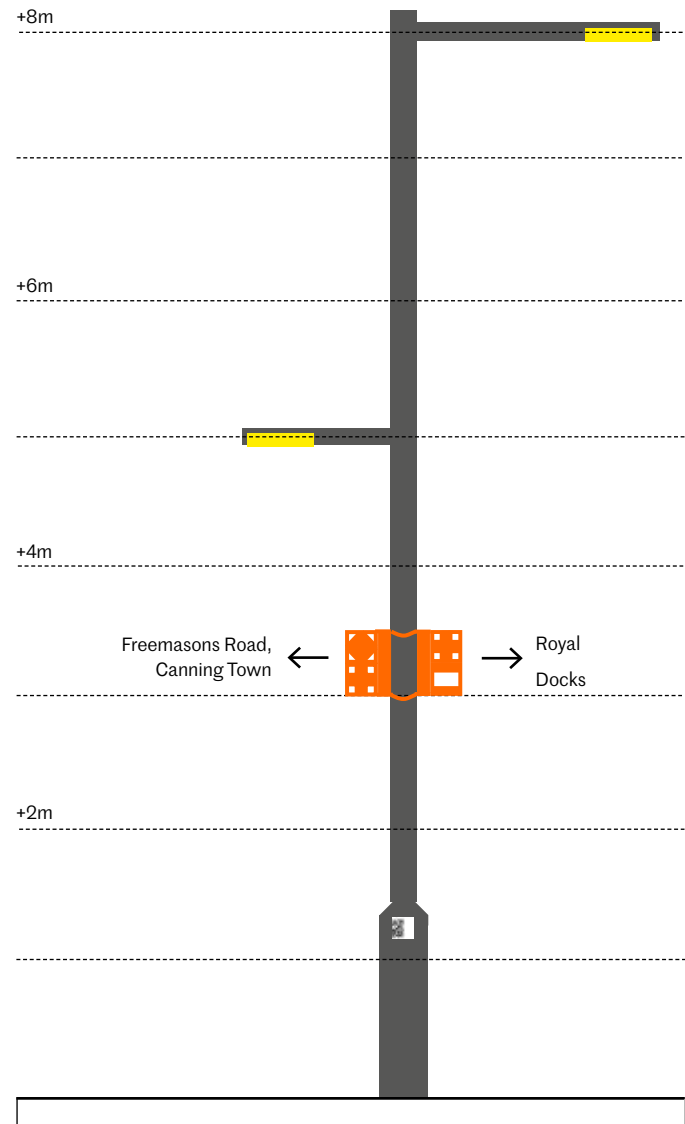
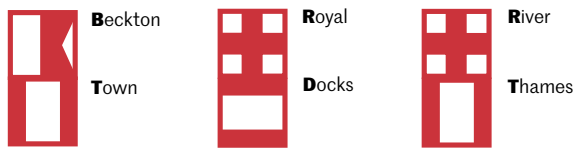
### 2 - Custom House to the Thames



### 3 - Connaught Crossing



### 4 - Beckton to North Woolwich



Stitch marker setting out diagram

## Perches

Bespoke perches form part of the seating family to be employed across the whole of the docks. Perches can aid in marking the Stitches by picking up the stitch colour code through the use of different aggregate mixes. For more information on the perches, refer to the Landscape Design Guide.



1 - Canning Town to the Docks  
2 - Custom House to the Thames  
3 - Connaught Crossing  
4 - Beckton to North Woolwich

## Innovation

QR codes included on lighting posts and wayfinding structures linking to area history and maritime codes to support creative wayfinding.



Example of Stitch marker on Stitch 4, Beckton to North Woolwich, mounted on a lighting column outside the foot tunnel entrance.



Perches proposed along the dock edge on Stitch 2, Custom House to the Thames, supporting the identity through material colour alongside other wayfinding elements.



# Area signs

Although there are distinctive landmarks in certain areas, large sections of the dock edge lack any significant public realm features that respond to the scale and character of the place. The provision of bespoke wayfinding signs, acting as mini landmarks, will help to break up the continuity of the dock edge, while also signalling your arrival at the water from the adjacent neighbourhoods.

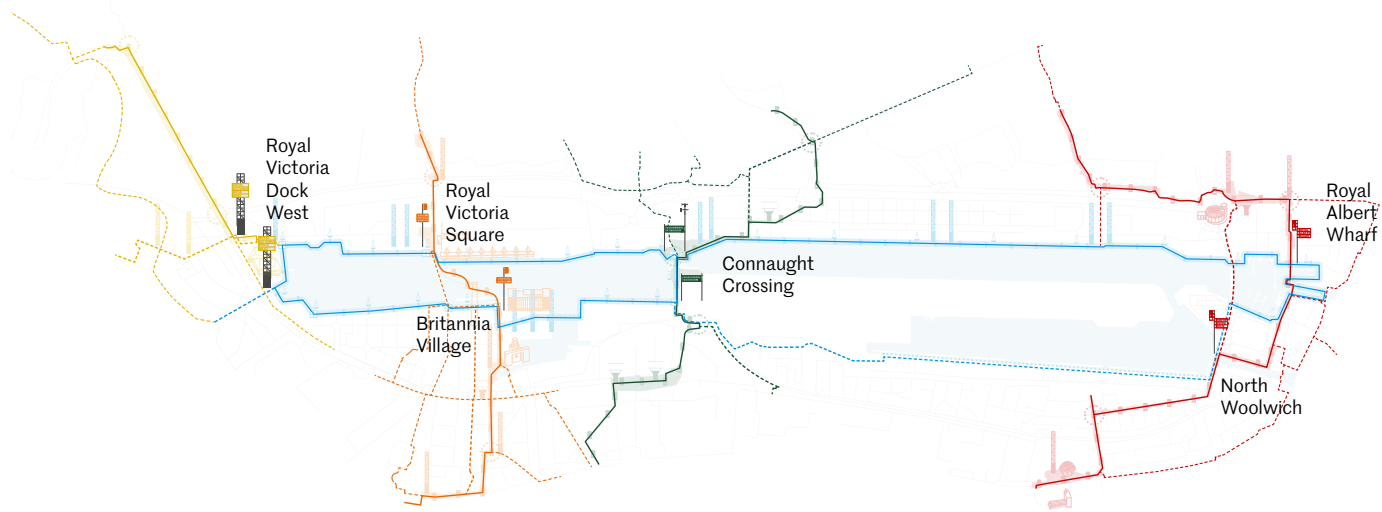
The proposed location of the Area Signs identify the key crossing points that connect neighbourhoods either side of the dock water. They also act to highlight key entrances to the Docks and should be positioned to have the most visual impact. There should be eight area signs in total, located either side of the dock water along the stitch routes. The development of the area signs should follow the design brief outlined below:

### Design Brief

The design of the area signs should be developed to consider the following:

- **Form** – The area signs should be free standing, supported by posts or columns.
- **Colour** – be colour coded to differentiate between each stitch.
- **Scale** – scaled appropriately to each site, to have visual impact from approaches and be easily identified to create clear and distinct place markers.
- **Character** – designed to reflect the maritime history of the Royal Docks.
- **Process** – be developed in close collaboration with local stakeholders and members of the community.

### Area signs plan



Greenway signage post, London

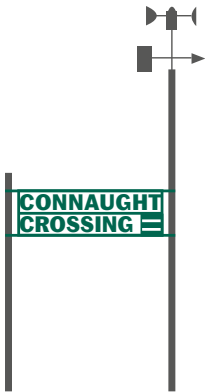
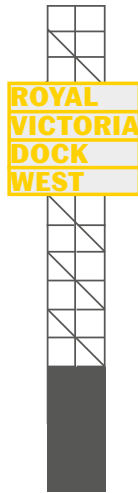


'Our Lady of Canvey', the Stony or Deadman Beacon on Canvey Island



Beacon structure on Twelve Trees Bridge, London

\*Area sign icons are indicative of the final design. This design guide sets out the principles for a more detailed design brief.



**1 - Canning Town to the Docks**  
The area signs located on Silvertown Way to the north and Crystal Gardens to the south mark the entrance points to the Royal Docks' western gateway. The signs should be clearly visible from the highway and be designed as a prominent visual landmark.



**2 - Custom House to the Thames**  
The area signs located at the ExCeL centre to the north and Britannia Village to the south, signal the new crossing point next to Royal Victoria Bridge. The signs should be designed to form mini-landmarks along the dock loop and be integrated with other public realm elements.



**3 - Connaught Crossing**  
The areas signs located either side of Connaught Crossing mark the eastern end of Victoria Dock. The signs should be designed at the human scale with taller measurement instruments installed above reflecting the site's exposure and urban wild character.



**4 - Beckton to North Woolwich**  
The area signs located on Royal Albert Wharf to the north and Bascule Bridge, North Woolwich to the south indicate the eastern edge route through Albert Island. The signs should be designed to reflect the nautical character of the marina and connection to the River Thames.

### Innovation

Digital signage - live data on transit arrival and departure times, walking times, nearby transit routes, and the availability of shared bikes, cars and scooters.





# Public space beacons

Due to the vast scale of the docks, high level beacons that can be seen at a distance should be used in public spaces to aid in wayfinding for both visitors and residents of the area.

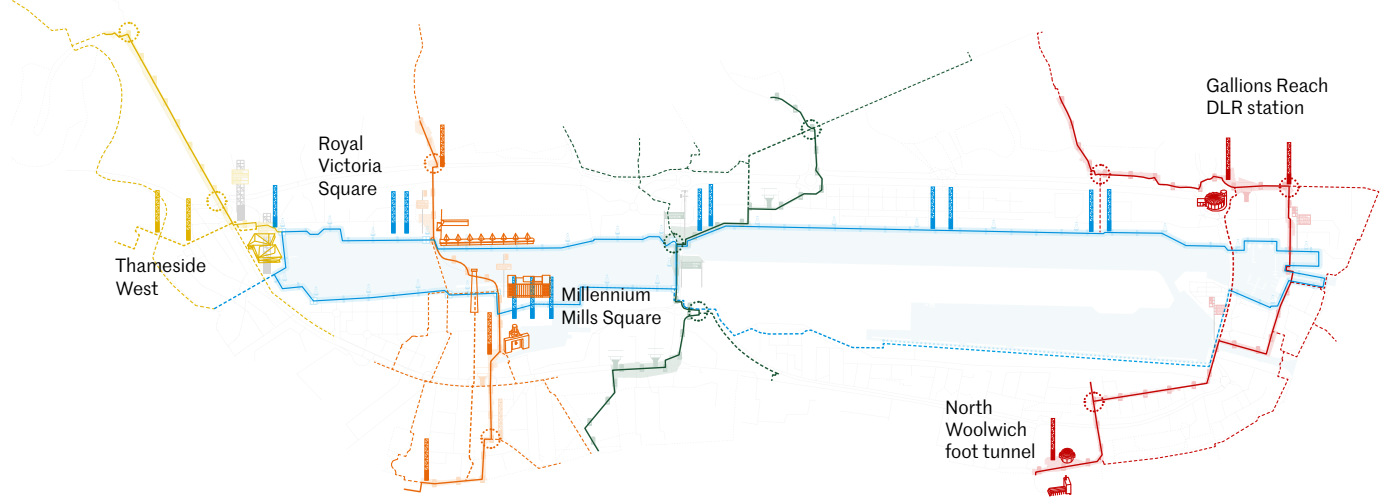
The lattice column is drawn from historical precedents around the dock edge and compliments quayside structures such as Victoria Dock cranes.

Lattice columns should be located in larger scale public spaces. They can be grouped as a family of columns or located individually. Lattice columns should be positioned for ease of access and maintenance.

Lattice columns should have a robust design and material, requiring little maintenance. Lattice columns should have a base panel upto 2.5m to deter people from climbing the structure. Developers and local authority should source an off the shelf design. For lattice columns located on stitches, these could incorporate the stitch markers.

Lattice columns should integrate bespoke lighting fittings consistent around the Docks. Refer to the Lighting Design Guide for further details of beacon lighting.

## Public space beacons and high level elements plan



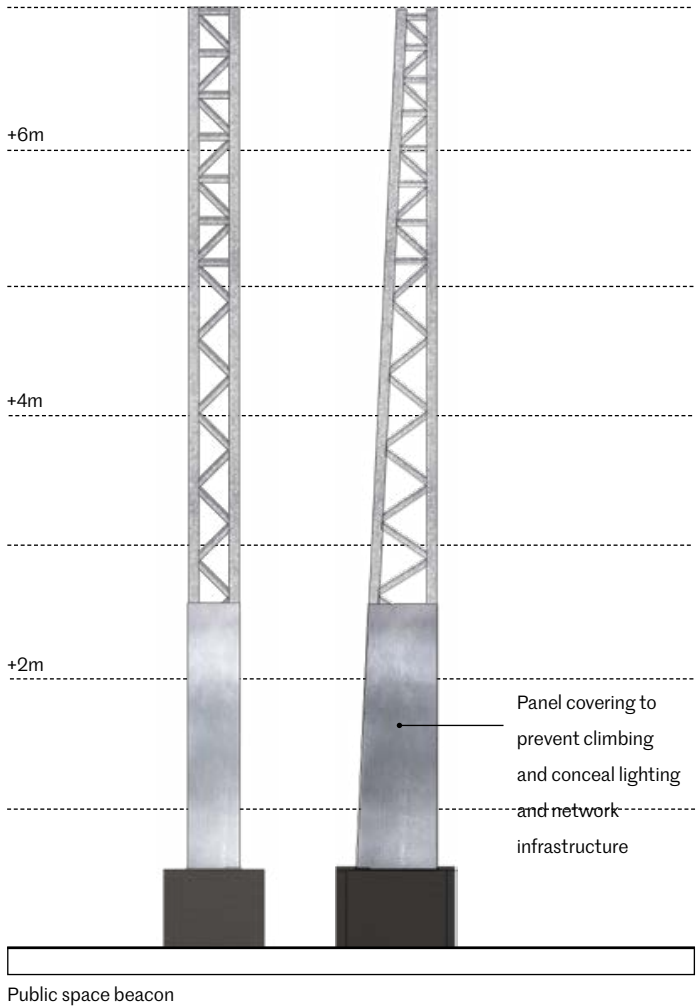
Lattice columns once present around the edge of Royal Victoria Dock



Lattice lighting column, Meridian Water Station, London



Lattice column manufactured by Tecnopali



### High level elements

In addition to the public space beacons, high level elements such as wind measuring instruments and symbols relating to maritime navigation should be used to aid in wayfinding over long distances as well as creating dynamic landmarks that people can interact with.

High level elements should be integrated with the existing urban fabric and street furniture where possible to reduce the amount of physical infrastructure in the public realm. User's of this guide should consider the character of the location and purpose of taller interventions when adopting this approach, working closely with the London Borough of Newham to identify appropriate mounting structures.

In many cases, the existing fabric of the Royal Docks provides a platform or structure that can be used as a wayfinding feature. For guidance on how supergraphics can be used to co-opt existing structures, refer to the Special Treatments palette in this document.



Lattice columns along the dock edge



Small cell network infrastructure unit

## Innovation

Integrate technology in wayfinding structures such as power/water/wifi/ EV charging and host small cell network infrastructure.

## Innovation

Beacons to include measurement instruments to monitor air quality, wind speed, temperature, etc to provide visual data.



The Northfleet lower light, 1931



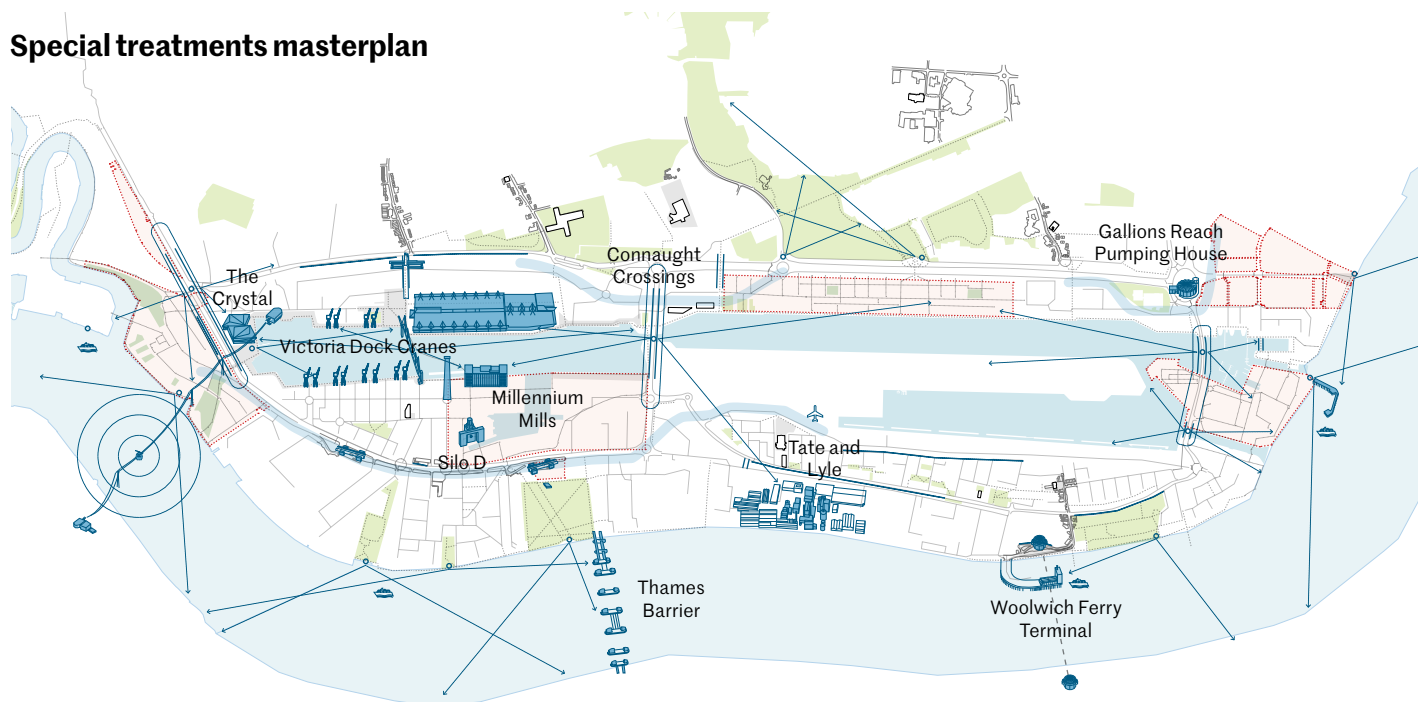
Beacon structures integrated with lighting column and wayfinding structures



# Special treatments

The use of 'special treatments' will play an important role in creating vibrant and impactful features that incorporate the existing urban fabric into the wayfinding strategy for the Royal Docks. The diverse number of sites and structures that range from unique heritage to impressive infrastructure alongside more everyday streetscape elements offer a particular opportunity for wayfinding in the area, using a range of tactics to compliment the baseline and accent palette approaches, and provide navigational features on the grand scale of the Royal Docks landscape.

Special treatments masterplan





# Places

The Royal Docks has a wealth of iconic landmarks - remnants of its industrial past and the ongoing development taking place - that in themselves contribute to a greater comprehension of the area. Expansive view points in turn offer a real sense of drama and exposure across the Docks. These long range vistas play a crucial role in understanding the scale of the place by framing landmarks and other landscape features.

The following pages offer guidance on how these landmarks and views can be exploited to compliment the range of tactics in the baseline and accent palettes.

## Landmarks

Prominent landmarks should be incorporated into the wayfinding strategy, either by sensitively restoring and protecting the structure itself or by enhancing it through bespoke treatments such as supergraphics or feature lighting. Both approaches should contribute to the landmark's character and overall quality of the area, alongside offering a greater sense of navigating the complex, super scale of the Docks.

User's of this guide should work with the local authority, key stakeholders and community groups to establish the correct treatments of different landmarks across the Royal Docks.

## Views

Dramatic long range views across the dock water, River Thames, parks and highways are an essential experience of the area. These views should be protected and enhanced through the ongoing development of the public realm.

Attention should be made to not obstruct views with public realm elements such as signage and street furniture. Where appropriate, the public realm could serve to better identify key viewpoints by marking, framing or opening up the public realm for the increased sense of exposure and scale of the landscape. An example of this can be found in the boundary treatment of the dock edge detailed in the Landscape Design Guide, Special Treatments palette.



The Thames Barrier as views as viewed from Thames Barrier Park



The Crystal, Royal Victoria Dock West



View from Royal Victoria Bridge west back towards the Crystal and City of London

## Existing structures for special treatments

The Special Treatments masterplan identifies possible sites and structures to co-opt into the wayfinding strategy. User's of this guide should identify sites on their estate and follow the principles for bespoke treatments detailed in the following pages.

Possible sites could include prominent features in the area, structures that have a vertical presence, but not exclude things like site fencing, street furniture and blank walls that have a linear presence at the scale of the pedestrian.

The examples here are illustrative of the diverse range of physical elements already present in the Docks. User's of this guide should assess the type and character the site, as well as the condition and function of the surrounding area, to determine the best approach.

## Infrastructure



DLR viaduct and Gallions Pumping Station



North Woolwich Ferry terminal



Royal Victoria Bridge



Custom House DLR station

## Streetscape



Boundary walls



Commercial buildings



Silo D



Silvertown chimney



Fencing



Street furniture



Lightship 93



Cranes

## Heritage



# Co-opting existing structures

The use of existing structures for wayfinding can reduce the amount of public realm clutter, providing large platforms for inventive and characterful projects in making the Docks an enjoyable place to be. Proposals to co-opt existing structures should in principle enhance the local area and the rich heritage of the place, creating distinctive landmark features to help people navigate the area. Potential sites for supergraphics could include:

- Landmark buildings.
- Infrastructure; bridges, viaducts, terminals.
- Heritage structures; cranes, artefacts.
- Boundary walls.
- Industrial / commercial building walls.
- Construction hoarding.
- Fencing.
- Street furniture; handrails, seating.

Proposals for supergraphics could take the form of one of the following approaches:

- Branding identity.
- Artist commissions.
- Community murals.
- Site / Distance marking.

Commissioners of projects should aim to support local artists and work with community groups, education institutions and key stakeholders in identifying sites and establishing briefs for supergraphics within the Docks. Any proposals put forward for supergraphics should be designed in accordance with the following criteria:

- Provide a landmark which people can identify and use to navigate.
- Enhance the quality and distinctiveness of the local setting.
- Aim to transform existing structures responding to the Docks' unique character.
- Do not have to be limited to one single area.
- Take into account the maintenance and management of the proposed supergraphics as part of the design process.



Blank wall outside North Woolwich foot tunnel used for landmark artwork / mural celebrating the area



Numbered cranes along the dock edge



Stitch colour to DLR viaduct Pier on North Woolwich Road



Artist commission to industrial buildings on Silvertown Way



Directional supergraphics to Connaught Crossing bridge

This page highlights three approaches for supergraphics within the Docks. The following page provides guidelines for public art, which elaborates on co-opting existing structures. For lighting special treatments of existing structures, refer to the Lighting Design Guide.

## Placemaking

To respond to the superscale of the Docks, large format type could be applied to tall built structures to create beacons in the area and contribute to enhancing the identity of the Royal Docks' neighbourhoods.

Type used should be of a design with clearly defined lettering following the place or structure name. Colour should be neutral, except where the structure sits on a stitch in which case it should adopt the stitch RAL colour.

## Local Data

Supergraphics in local centres and residential neighbourhoods should reinterpret particular qualities of that area. Graphic information, produced in close collaboration with the community, could be applied to un-activated elements of the streetscape such as rail boundary walls to reveal aspects of community life, geography, history, etc, which otherwise might be hard to detect.

The use of local data is a tool for grounding wayfinding interventions in the context of the area, providing greater insight and appreciation when navigating the public realm.

## Colour supporting the Stitches

Alongside interventions in prominent areas such as public squares and transport interchanges, supergraphics should be deployed to assist wayfinding along the Stitches.

Along stitch routes, colour could be applied in powder coating to key structures and street furniture. The application of colour will improve existing streetscape elements, such as road barriers and park fencing, or highlight prominent route features such as bridge structures and other high level elements - colour should not be applied wholesale to all public realm elements.



Type / Tate, Liverpool



Type marking West Silvertown to existing chimney



Local data as mural to DLR wall on Albert Road, North Woolwich

- Survey of retail trade painted on wall in Wood Street, London



Victoria Gardens railing painted with Stitch 4 RAL colour

- Painted bridge structure on the Wirral, Liverpool



# Public art

The use of public art can help to define the character of particular places, supporting wayfinding around the Royal Docks. Public art in principle should enhance the local area and the rich heritage of the Docks, creating distinctive landmarks to help people locate specific places.

Commissioners of projects should aim to support local artists and work with community groups, education institutions and key stakeholders in identifying sites and establishing briefs for public art pieces within the Royal Docks. Where possible, local businesses should be part of the process, playing a role in the fabrication and installation of artworks.

Public art should be considered across a wide range of settings including, but not restricted to, public areas where people gather. The location and positioning of public art should not overly impinge footways or obstruct movement. Any proposal for public art should not significantly increase the existing maintenance regime for the area.

## Local heritage

Public art could be used to recognise and celebrate the important role the people of the area have played in its development, grounding public realm improvements within the communities which will be experiencing them.

The statue of the dock workers outside the ExCeL Centre is a good example of this approach. A statue representing the women who worked at the Tate & Lyle sugar refinery would be equally welcomed by stakeholders and community groups.

Innovation

Links to social media and blogs celebrating communities' involvement in projects and new landscapes.

## Design Brief

Public art should be designed in accordance with the following criteria:

- provide a landmark which people can identify and use to navigate.
- enhance the quality and distinctiveness of the local setting.
- be raised on a plinth or have a tactile surrounding floor as a warn-off feature for blind or partially sighted people.
- aim to transform objects / sites responding to the docks' character.
- not be limited to single objects or expensive materials.
- take into account the maintenance and management of public art as part of the design process.

Planning consent may be required from the local authority depending on the size, location and function of the proposal. Further information on delivering public art can be found in the TfL Streetscape Guidance 2019 (<http://content.tfl.gov.uk/streetscape-guidance-.pdf>).



Statue of dock workers outside the ExCeL centre



Dock workers



Sugar refinery workers

## Maritime theme

As well as commissions for new, bespoke public art pieces, the deployment of public art using maritime objects in the public realm is also greatly encouraged. In some areas, like the dock edge, it will be appropriate to have multiple items placed along the route. User's of this guide should consider integrating them with other elements, such as planters, rather than placing them in isolation where they will have less impact.

Maritime objects should be lit at night, and if they are placed on a stitch it may be appropriate to utilise the stitch colour if it does not overwhelm the streetscape. Relevant objects could include mooring posts, buoys, winches, anchors, signs, markers, flags, lanterns as well as heritage boats brought to the dock edge.

Innovation

Mobile infrastructure - small cell data integrated into public art structures.

## Creative commissions

Alongside static pieces in prominent areas of the public realm, public art should be deployed in a more playful and expressive way. This could include bespoke treatments of moving objects, such as the North Woolwich Ferry or the Emirates Airline cable cars. Artists could also be welcomed to create thoughtful co-opting of infrastructural elements, such as viaducts, cranes and walls, and other elements that portray the operational qualities of the Royal Docks history and present condition.



Trinity Buoy Wharf, London



Dock buoys and cranes have a distinctive character recognisable to the Docks' landscape. Public art can play a role in uplifting elements like these into the programme of public realm improvements.



Peter Blake dazzle ship, Liverpool





The following chapter describes the wayfinding proposals in six worked examples taken from across the Royal Docks. Each worked example has been masked to reveal the public realm elements applicable to the wayfinding treatments outlined in the palette section.

The worked examples are indicative of how the palettes combine to create a characterful, clear and inclusive public realm. Users of this guide should use the worked examples as references for how the palette components could be deployed on their site.

This chapter should be read in conjunction with the accompanying lighting, landscape and accessibility and inclusive design guides which make up the Royal Docks public realm designers' pack.

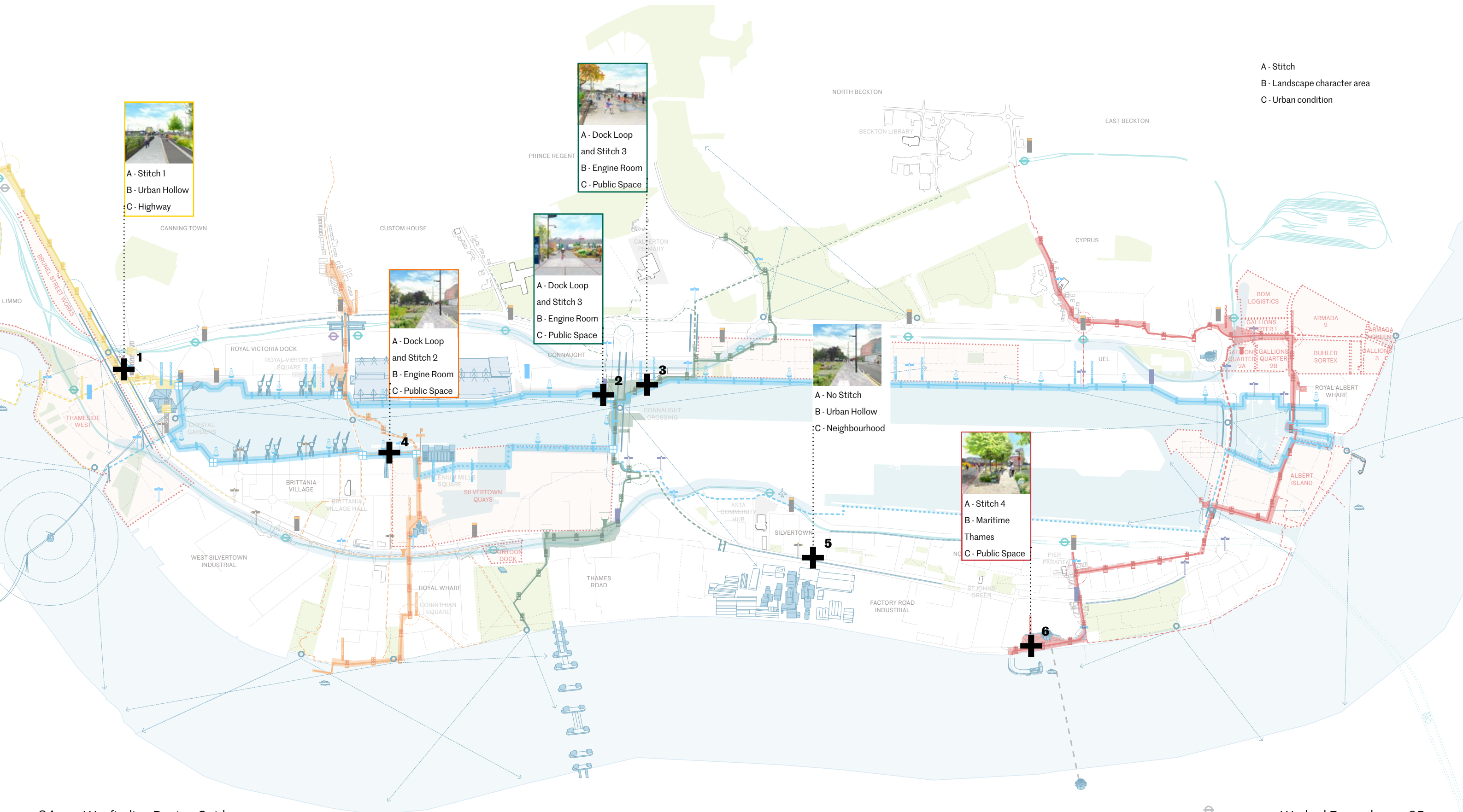
## 4. Worked examples



# Worked examples

The worked examples have been taken from a cross section of locations across the Royal Docks that best illustrate the range of public realm approaches proposed in the design guides. User's of this design guide should refer to the worked examples as illustrative views of how the palettes might be applied.

The following pages provide detailed worked examples of six locations, showcasing the range of public realm proposals with specific focus on the wayfinding elements. Please refer to the landscape and lighting design guide for further information on the respective approaches.





# 1. Silvertown Way

This stretch of the Silvertown Viaduct sits on the Leaway to the Docks stitch, and is characterised by being elevated above its surroundings. As part of the reconfiguration of North Woolwich Road / Silvertown Way, the viaduct will be rebalanced to become far more hospitable to pedestrians and cyclists. The wayfinding treatment reflects its new status as an accessible and welcoming route from Canning Town and the River Lea to the docks.

Wayfinding proposals here centre around drawing people to the docks, with a cluster of elements at the junction with Tidal Basin road. Characterful area signs and painted structures at lower level help the viaduct feel like an 'urban pier', with curious objects and signs rising up from the lower level of the docks. Stitch markers at regular intervals reassure users that they are on the right track.



Proposed treatment



Wayfinding elements isolated



# 2. Dock Edge

This stretch of dock edge, by Royal Albert Dock, can be reconfigured to provide greater access to the water as well as making better use of its large empty spaces.

The dock edge lighting columns provide visual consistency, replacing the current sphere fitting. The lattice lighting column, reminiscent of historic cranes and winches, clearly mark the broad public space besides the dock edge route. Maritime objects and painted structures, as well as the dock edge lighting column and the water itself, draw users along the dock to the east.





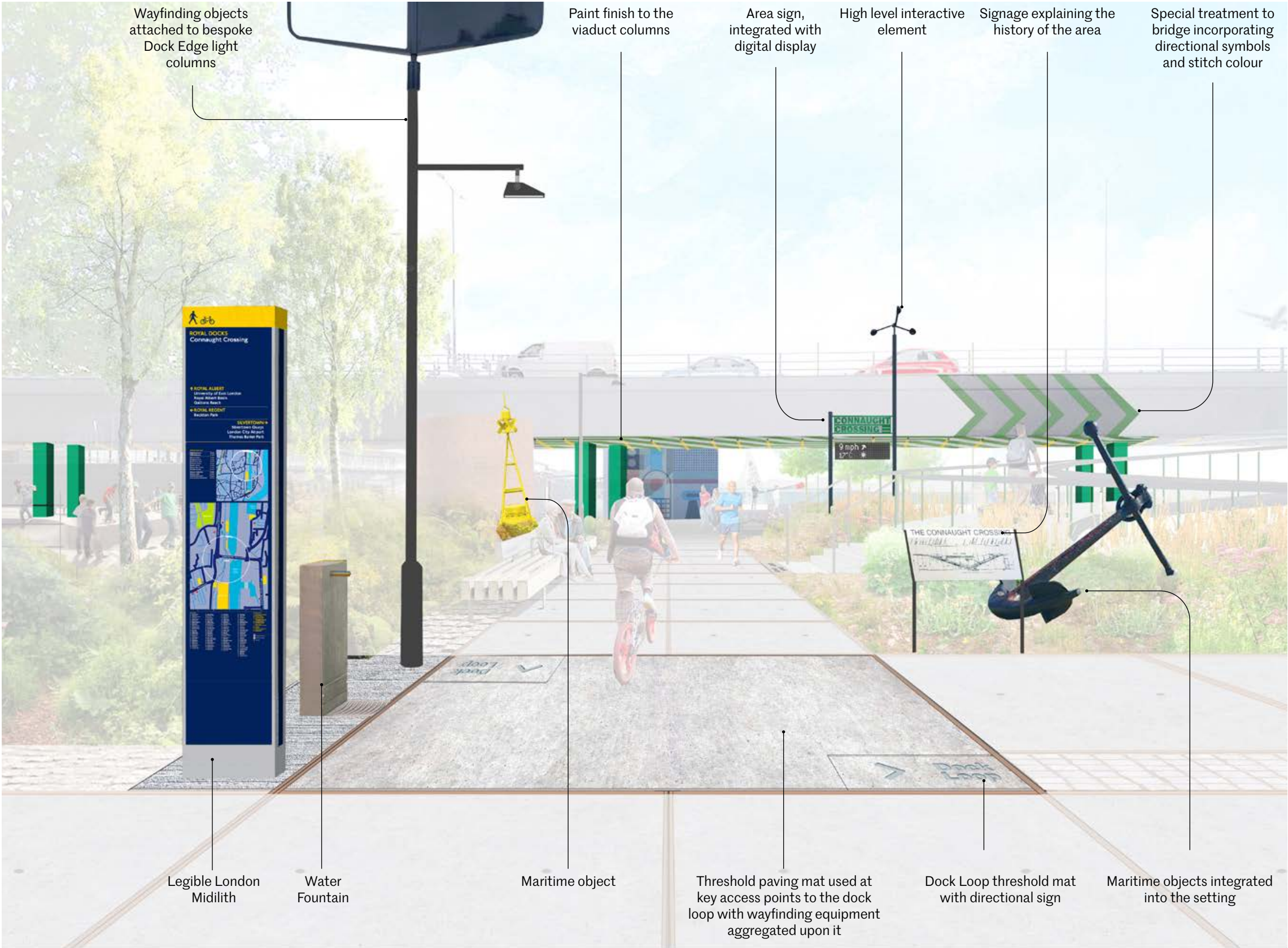
# 3. Connaught Crossing

Here, the confluence of the Connaught Crossings stitch and the Dock Loop creates an exciting setting for wayfinding interventions. As a key node, a Legible London midilith is proposed to help people navigate north-south, while characterful interventions play off the open and windswept character of the place.

Existing infrastructure is treated with colour and graphics to give character, and encourage people to cross the water and continue along the Dock Loop. The Dock Loop paver and route marker is shown, providing coherence to the route. Maritime objects are integrated into the landscape, enhancing the Engine Room character of this site.



Proposed treatment



Wayfinding elements isolated



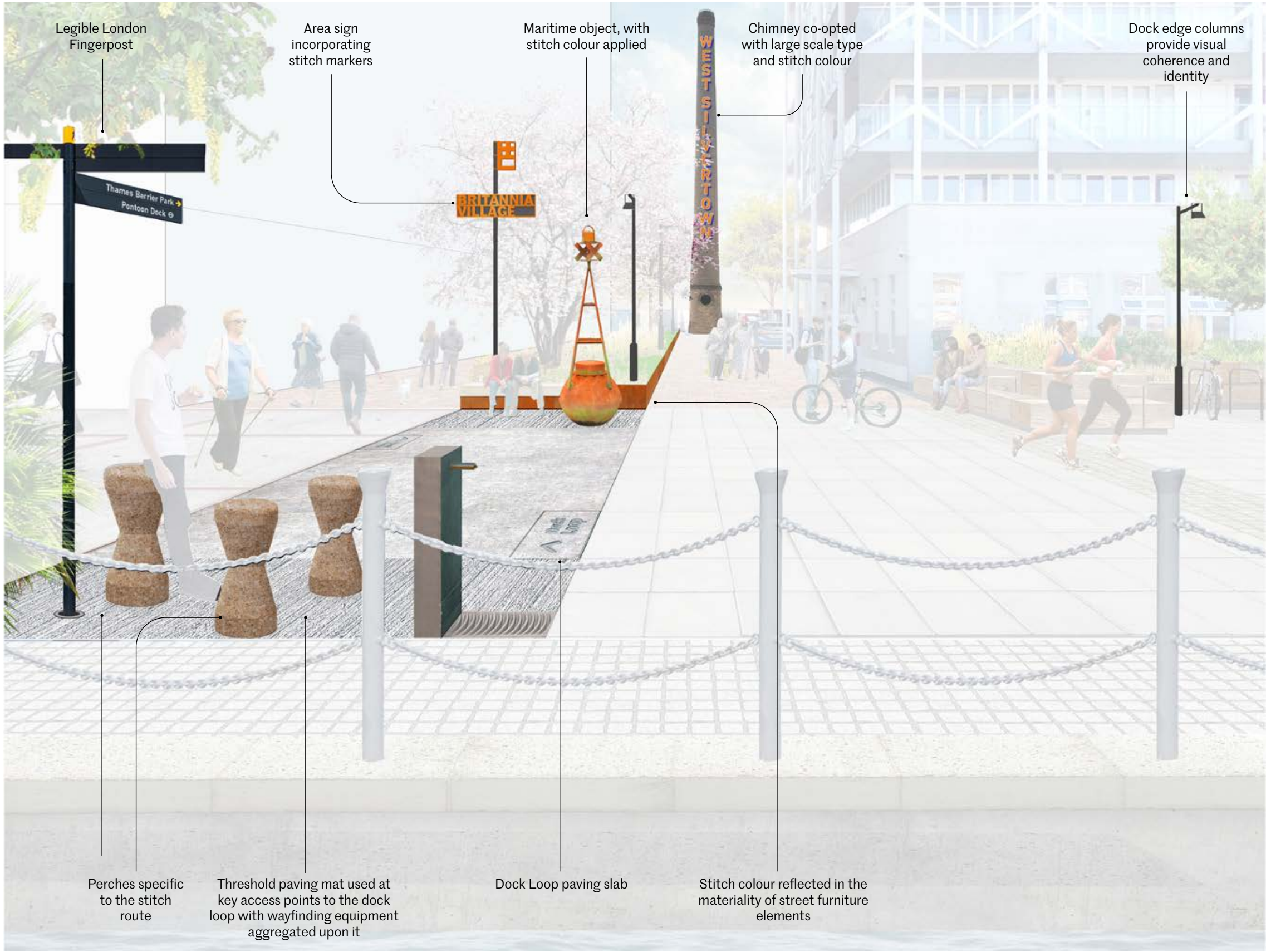
# 4. Britannia Village

This worked example shows where the Freemasons Road meets the Thames stitch and the Dock Loop. This is a meeting place for the dock, the existing community of Britannia Village and the proposed Silvertown development, which necessitates wayfinding and public realm elements that tie everything together in a coherent manner.

The application of the stitch's orange tone gives coherence, while characterful interventions like the area sign and maritime objects, help give a strong identity to the place. The Dock Loop paver draws people along the water, while the painted chimney helps guide pedestrians and cyclists towards North Woolwich Road and the Thames.



Proposed treatment



Wayfinding elements isolated



# 5. Albert Road

Albert Road does not lie on a stitch, however there are well established communities and well-used public spaces in the area. Wayfinding here is therefore more focused on providing amenity and character to these rejuvenated spaces. Community notice boards, murals and characterful interventions to existing structures such as the Crossrail wall and the Tate and Lyle refinery all serve to enhance the identity of this area.

Decluttering the streetscape is a priority, and the provision of community notice boards should help aggregate useful information while minimising clutter in the wider area.



Proposed treatment



Wayfinding elements isolated



# 6. Thames Edge

The approach to the Woolwich foot tunnel and ferry is an important location for continuing journeys over the Thames - this is one of only two crossing points in the Royal Docks, the other being the Emirates Airline.

There is an opportunity to improve the public realm here with planting and benches, and this can be supported by characterful murals added to areas of blank wall. The stitch markers will help pedestrians and cyclists navigate their way towards Gallions Reach, while Legible London features ensure that a granular level of wayfinding is supported.



Proposed treatment



Wayfinding elements isolated





The following chapter provides more detailed design information on selected components from the wayfinding palette. The design information contains useful guidance on the design, manufacturing, procurement and maintenance of the selected components.

This chapter should be read in conjunction with the accompanying lighting, landscape and accessibility and inclusive design guides which make up the Royal Docks public realm designers' pack.

## 5. Design Information



# Dock Loop paving slabs

refer to palettes, chapter 3, The Dock Loop.

The Dock Loop paving slabs identify the Dock Loop stitch. The paver has been designed to integrate with the existing hard surfacing most commonly found along the dock edge.

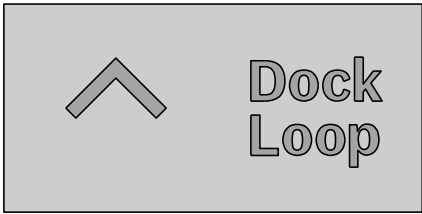
The paver has three styles, to be used in different locations along the dock edge. The pavers with Dock Loop lettering are only to be used along the Victoria Dock Loop. The identity sign, incorporating the maritime flags from the Royal Docks' brand identity, can be used in all dock edge areas.

### Slab specification

- 440 x 885mm, nominal thickness 75/150mm (to be confirmed by structural engineer, depth will be determined by expected vehicle loadings).
- Natural colour concrete with standard surface finish.
- Must meet required PTV value.
- All cast in lettering to be 5mm deep.
- All symbols to be either 3mm or 6mm deep.
- Installed on appropriate sub-base which will be determined by ground conditions and potential vehicular loadings.
- Ground condition surveys must be commissioned, especially on dock edge where false quays may limit slab depths.

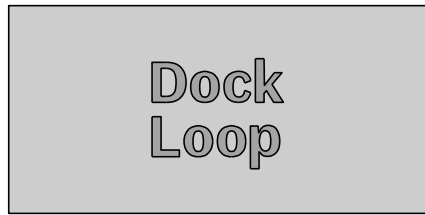


### Directional



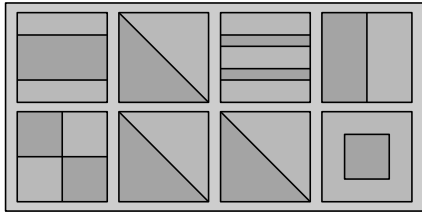
To be used where the route is not apparent and further direction is required

### Route

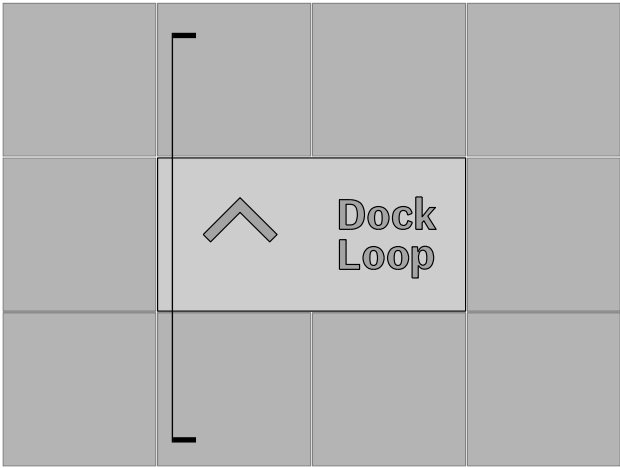


To be used periodically along the route to give identity

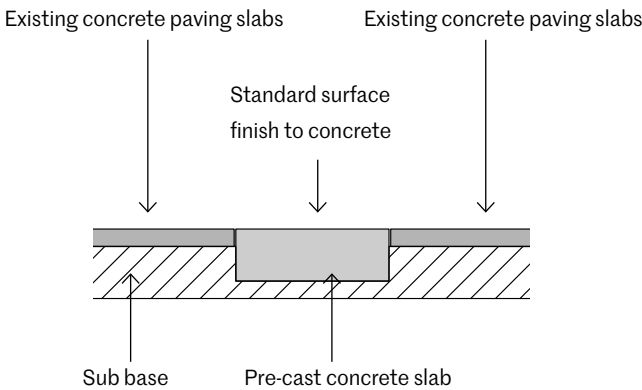
### Identity and accent



To be used periodically along the route to give identity, as well as to give accents to active mats



Dimensions of slabs - two paving slabs wide (plus slab gap width) x one paving slab tall



Depth to structural engineer's requirements - will depend on whether vehicles will use route at any time



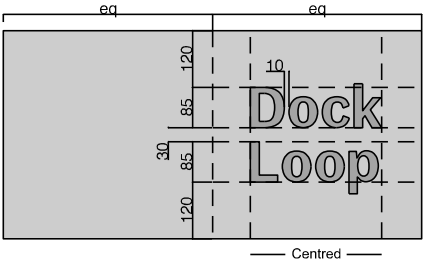
# Dock Loop paving slabs

## Directional slab

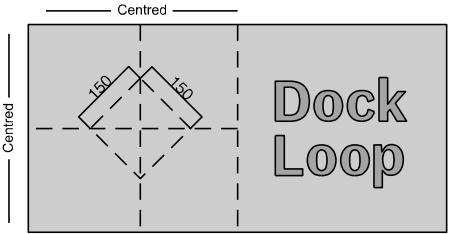
1. Standard slab dimensions:  
885mm x 440 mm (depth to Structural Engineer's specification)



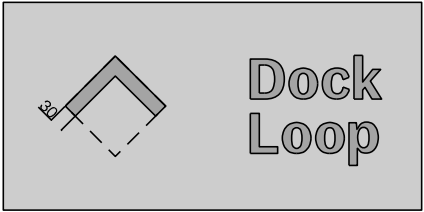
2. Construct text grid and arrange text, 10mm spacing between letters with first letter of each line capitalised. Text to be 5mm deep



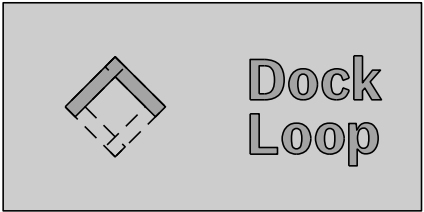
3. Construct arrow grid, 150mm square centred on the left-hand half of the slab



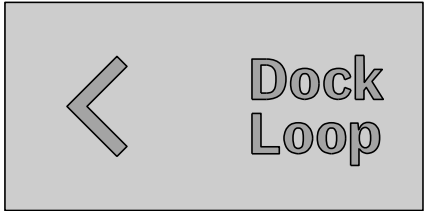
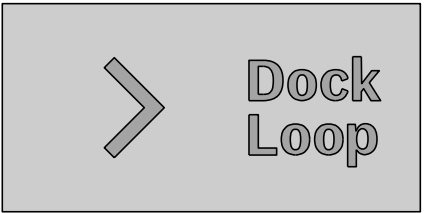
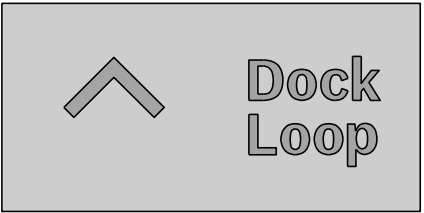
4. Arrange arrow-head as required, 30mm offset from edge of grid. Arrows to be 3mm deep



5. Arrows can be rotated around the centre of the grid - arrows should never point backwards/down



## Directional slab range

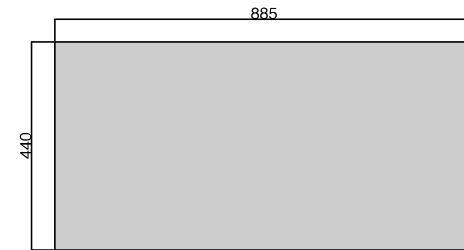




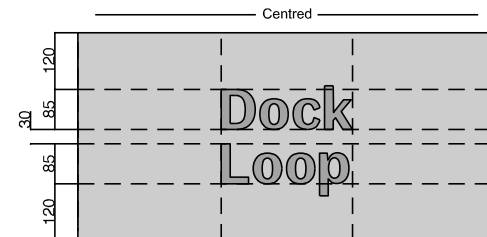
## Dock Loop paving slabs

## Route marker slab

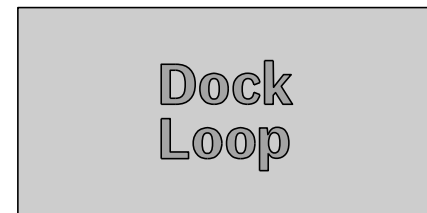
1. Standard slab dimensions:  
885mm x 440 mm (depth to Structural  
Engineer's specification)



2. Construct text grid and arrange text centred horizontally, 10mm spacing between letters with first letter of each line capitalised. Text to be 5mm deep.

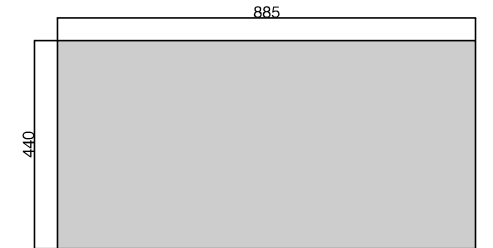


### 3. Completed slab

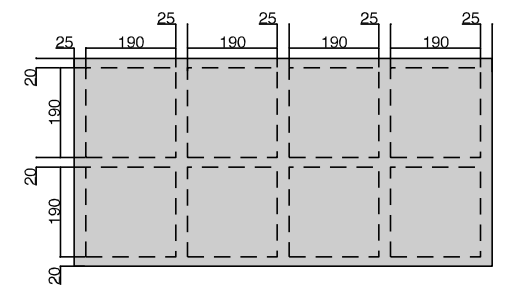


## Identity slab

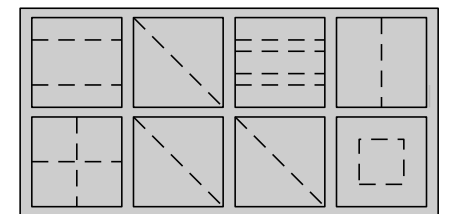
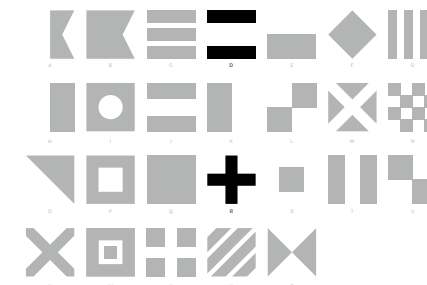
1. Standard slab dimensions:  
885mm x 440 mm (depth to Structural  
Engineer's specification)



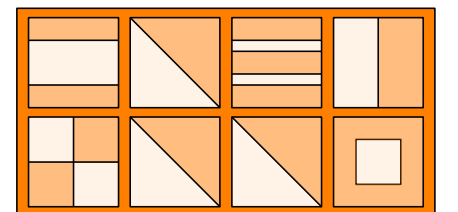
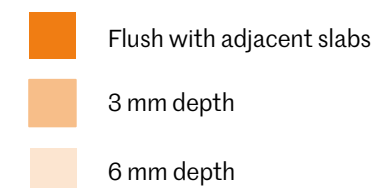
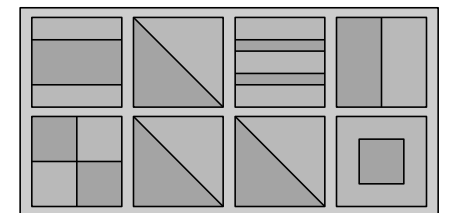
2. Construct flag grid, 190mm squares set apart 20mm vertically and 25mm horizontally



3. Arrange flag designs in order to follow Royal Docks branding. 'Dock Loop' is spelt out in code



4. Completed slabs (see key below for depth of relief for flags)





# Stitch markers

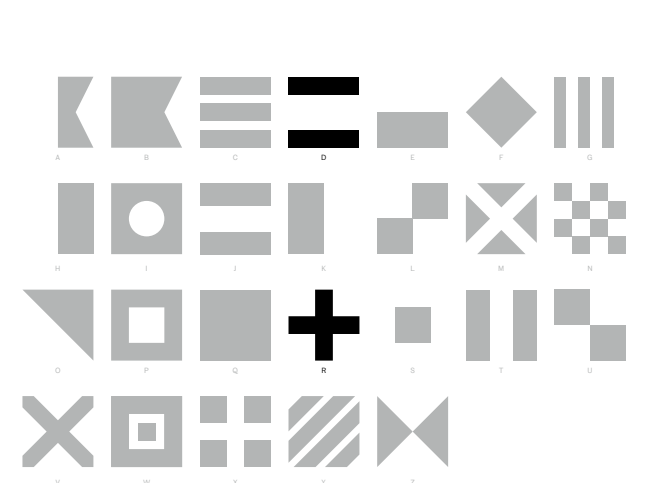
refer to palettes, chapter 3, Stitch markers.

The stitch markers identify the stitch routes through colour and the use of maritime flag symbols which form part of the Royal Docks' brand identity. Each marker is made up of two flag symbols cut into one metal sheet. The overall dimensions of the stitch marker sheet fit into standard sheet metal size for economical manufacturing.

The markers are to be installed on lighting columns and therefore require an adaptable mounting system. The bracket used to mount the sign and connect to the lighting column is a standard product which can be sourced from sign manufacturers such as AJ Wells. The bracket is fabricated from aluminium and should be powder coated to the same colour as the stitch marker. The component used to connect the bracket to the column are 1mm stainless steel straps, self finished.

The stitch markers have been designed for ease of manufacture and assembly. Overall assembly will require the input from a structural engineer to assess loading requirements of the mounting system and their stability as a unit. To install a stitch marker, the London Borough of Newham's lighting officer will need to be consulted to assess the suitability of the lighting column to be used.

## Concept design



The Royal Docks brand consultants 'dn&co' initially developed an alphabet from the maritime flags to use as the basis for the Royal Docks brand. The Stitch markers take this code to form the sign symbols.

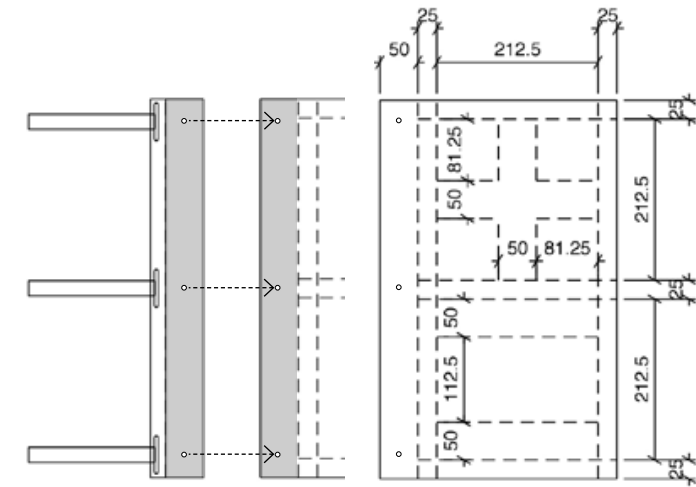
## Marker specification

- 312.5mm x 500mm wet cut mild steel marker sheet, nominal thickness 8mm (to be developed with manufacturer).
- Cut steel marker dimensions to follow detail design setting out and powder coated to one of four RAL colours depending on location.
- Aluminium bracket to manufacturers specification, powder coated to match cut steel marker, with stainless steel straps to connect to lighting column.
- Galvanised steel rivets fastening cut steel marker to mounting bracket.
- Install.
- Structural assessments of lighting column loading capacity must be carried out before installation.

## Maintenance

The stitch markers will require little ongoing maintenance after installation due to the durable powder coating to both the cut metal sheet and bracket. The condition of the markers should be inspected every so often as they may require cleaning. In the event of damage, stitch markers should be taken down to assess and either repaired or replaced depending on the damage.

## Detail design



Establishing formatting rules for the Stitch markers and how the cut steel sheet is fastened to the bracket. The following pages detail how each marker is to be set out for fabrication.

## Stitch marker range and setting out on site

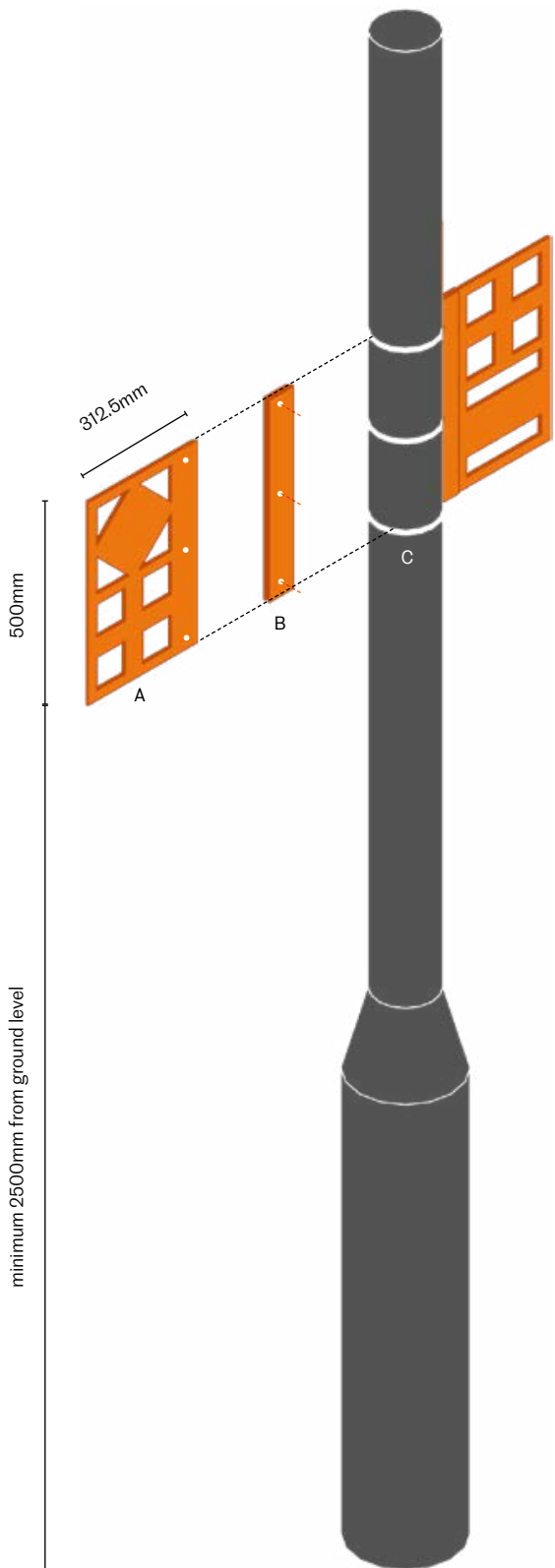
### Stitch

	1 - Canning Town to the Docks	2 - Custom House to the Thames	3 - Connaught Crossing	4 - Beckton to North Woolwich
	<b>RAL 1003</b> Signal Yellow	<b>RAL 2004</b> Pure Orange	<b>RAL 6026</b> Opal Green	<b>RAL 3020</b> Traffic Red
Newham north				
Royal Docks				
River Thames				

Direction towards:

Range of stitch markers to be used across the Docks. RAL colours to be applied to stitch marker and mounting bracket.

## Stitch marker range and setting out on site

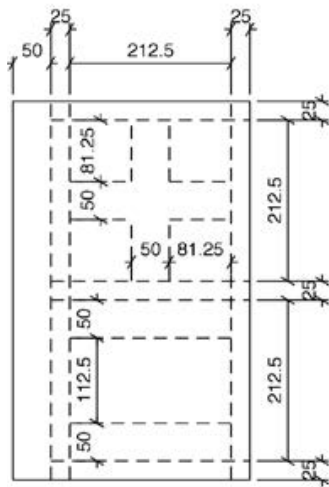


Assembly and setting out on site:  
A - Stitch marker  
B - Mounting bracket  
C - Mounting straps

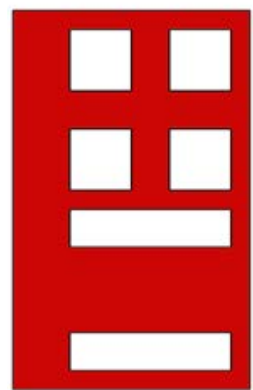
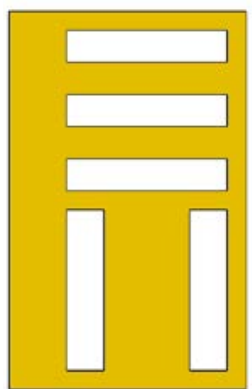
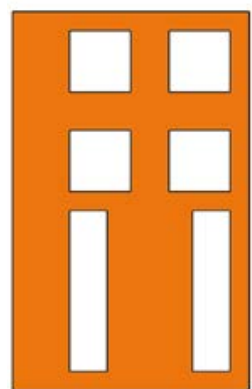
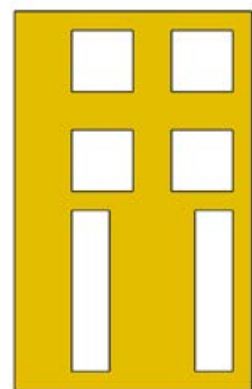
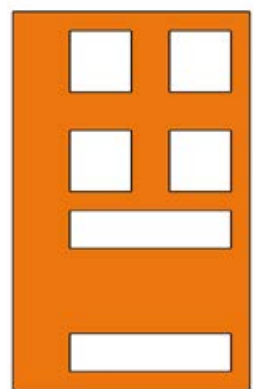
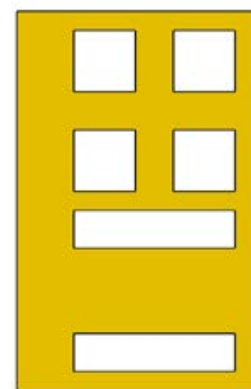
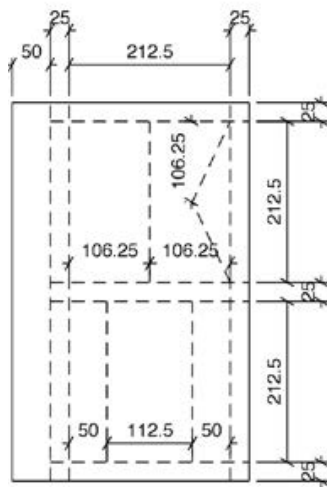
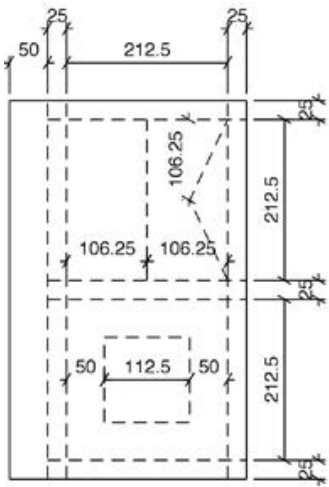
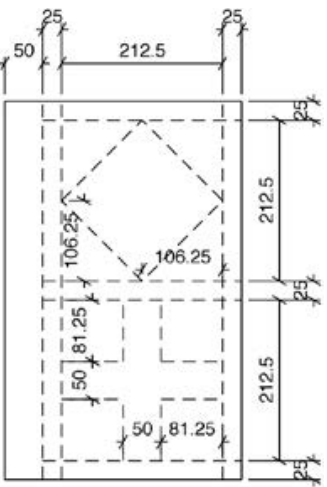
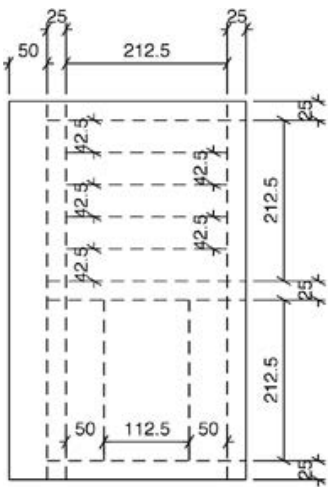
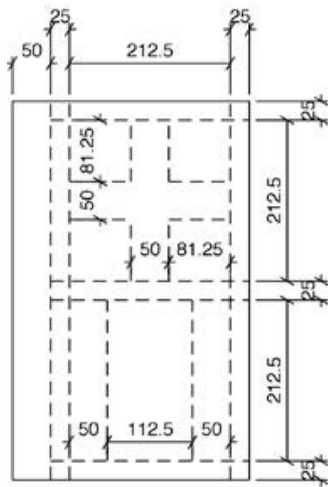


# Stitch markers

Royal Docks marker  
To be used in locations either side of the dock water, with the sign installed in the direction of the Docks.



River Thames marker  
To be used in locations between the dock water and River Thames, with the sign installed in the direction of the River Thames.



Newham north markers  
To be used in locations between the dock water and the neighbourhoods to the north, with the sign installed in the direction of the northern neighbourhoods.

- 1 - Canning Town to the Docks
- 2 - Custom House to the Thames
- 3 - Connaught Crossing
- 4 - Beckton to North Woolwich
- 5 - Dock Loop



# Public space beacons

refer to palettes, chapter 3, Public space beacons.

Public space beacons, located in larger open areas, reflect the industrial heritage of the Docks and act as vertical landmarks to navigate between. The design of the beacons takes cues from historical precedents once used in the Docks' operation as a busy trading port, adding character to the public realm.

Additionally, the lattice frame structure will act as a platform for other things, allowing for feature lighting to create landmarks after dark and providing the opportunity to host network infrastructure as the demands for mobile reception increase, exemplifying the Royal Docks as a place of innovative public realm design.

The lattice frame construction for the beacons is proposed so as to be easily manufactured and assembled on site, with the idea that they could be economically procured by a contractor in a short space of time. The developed design will require input from a structural engineer to determine the stability and loading requirements of the structure. The beacons will need to be located in areas so as to be accessible via a mobile work platform for installation and maintenance.



Historical precedent of lattice columns located on the Royal Docks.



Lattice column by Tecnopali



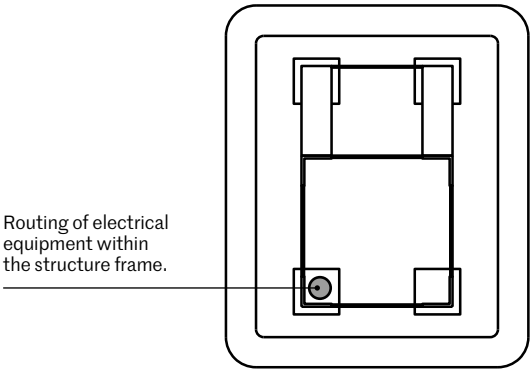
Internal lighting of the structure to provide landmark for wayfinding after dark. Crane, Dublin Port.

### Feature lighting

For more information on the lighting design of the public space beacons, please refer to the lighting design guide.

### Mobile network infrastructure

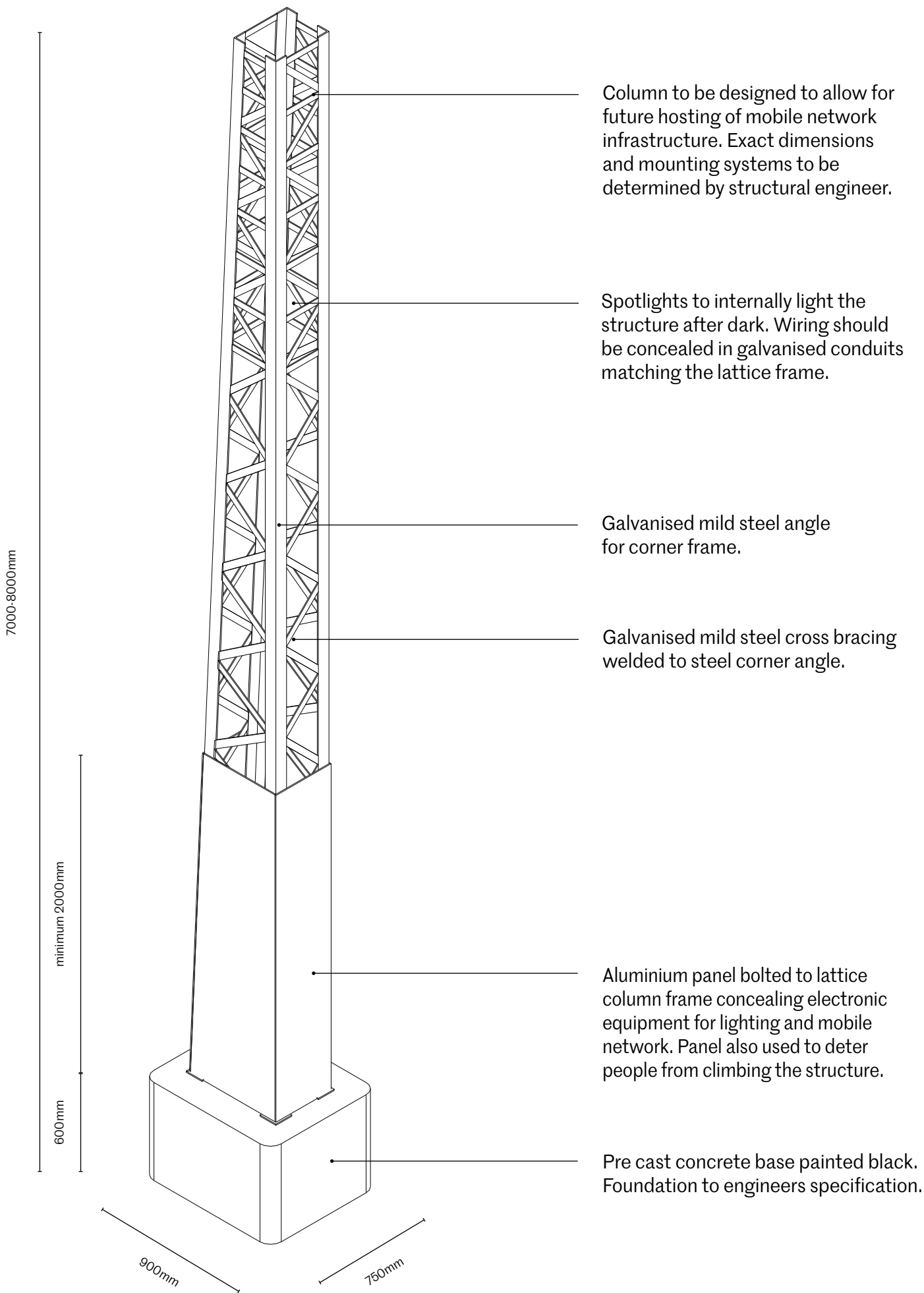
Technical information such as loading requirements and maximum deflection values to be confirmed by network provider and structural engineers.



Routing of electrical equipment within the structure frame.

Indicative plan of public space beacon

Exact dimensions to be determined by structural engineer and manufacturer depending on the loading requirements







# **A. Detailed masterplans**



**Wayfinding masterplan - Project codes**

The following pages provide detailed plans of the locations of public realm proposals set out in the Wayfinding Design Guide. The project codes should be read in conjunction with the detailed plans.

For guidance on the public realm proposals, refer to chapter 3, Palettes, For technical guidance on selected palette components, refer to chapter 5, Design Information.

Project Code	Project Description
W-BP-LL-01	Legible London fingerpost at junction of Thames Path and Thameside West bridge to Trinity Bouy Wharf
W-BP-LL-02	Legible London midilith in forecourt of future 'the Crystal' DLR station
W-BP-LL-03	Legible London fingerpost at entrance to Silvertown Way underpass and the Crystal
W-BP-LL-04	Legible London fingerpost at park in Thameside West development
W-BP-LL-05	Legible London fingerpost at south west perimeter of Thameside West development
W-BP-LL-06	Legible London fingerpost on North Woolwich Road at junction of Bradfield Road and Boxley Street
W-BP-LL-07	Legible London fingerpost on North Woolwich Road at junction of Cuunningham Avenue and the new route into Silvertown
W-BP-LL-08	Legible London fingerpost on dock edge path at junction of new Silvertown Quays bridge and Millenium Mills
W-BP-LL-09	Legible London midilith outside ExCel eastern entrance
W-BP-LL-10	Legible London midilith at Connaught Crossing north
W-BP-LL-11	Legible London fingerpost at end of footpath on Lynx Way
W-BP-LL-12	Legible London fingerpost at turning from Connaught Bridge to A112 east
W-BP-LL-13	Legible London midilith at end of footpath on North Woolwich Road
W-BP-LL-14	Legible London fingerpost at A112 and Connaught Road roundabout
W-BP-LL-15	Legible London fingerpost on A112 at turning to Hartmann Road
W-BP-LL-16	Legible London fingerpost on dock edge path at turning to footpath towards Dockside Road and Green Bridge
W-BP-LL-17	Legible London fingerpost at footpath junction north of Green Bridge
W-BP-LL-18	Legible London fingerpost at junction of Capital Ring footpath and Stansfield Road
W-BP-LL-19	Legible London fingerpost on dock edge path at turning to Royal Albert Quay towards Beckton Park DLR station
W-BP-LL-20	Legible London fingerpost outside Beckton Community Centre on East Ham Manor Way
W-BP-LL-21	Legible London fingerpost on dock edge path at turning to Captial Ring footpath toowards Cyprus DLR station
W-BP-LL-22	Legible London midilith on dock edge path at turning to Captial Ring footpath toowards Cyprus DLR station
W-BP-LL-23	Legible London midilith at junction of Albert Road and Pier Road
W-BP-LL-24	Legible London fingerpost on Capital Ring footpath junction with Pier Road north of North
W-BP-LL-25	Legible London fingerpost on Albert Road at turning to Woolwich Manor Way
W-BP-LL-26	Legible London fingerpost on Fishguard Way at junction of Woolwich Manor Way and Albert Road

W-BP-LL-27	Legible London fingerpost at south east perimeter of Gallions Roundabout north of Sir Steve Redgrave Bridge
W-BP-LL-28	Legible London fingerpost on dock edge path at junction of Albert Basin Way and Basin
W-BP-LL-29	Legible London fingerpost on Atlantis Avenue towards Gallions Reach DLR station
W-BP-LL-30	Legible London fingerpost on dock edge path at turning from Hudson Way to Lock Side Way
W-AP-S1-01	High level element outside Canning Town station on Silvertown Way
W-AP-S1-02	Stitch markers to be installed at regular intervals along route
W-AP-S1-03	Public space beacon at north west perimeter of Thameside West Development
W-AP-S1-04	Public space beacon next to future Thameside West DLR station
W-AP-S1-05	Area sign on Silvertown Way at junction with Tidal Basin Road
W-AP-S1-06	Supergraphics / feature lighting to the Crystal
W-AP-S1-07	Area sign on dock path south of the Crystal towards Silvertown Way underpass
W-AP-S2-01	Stitch markers to be installed at regular intervals along route
W-AP-S2-02	High level feature on Victoria Dock Road at junction with Freemasons Road
W-AP-S2-03	Public space beacon on Victoria Dock Road at junction with Freemasons Road
W-AP-S2-04	Area sign outside ExCel eastern entrance
W-AP-S2-05	Area sign west of Millennium Mills at at north west perimeter of Silvertown Quays Development
W-AP-S2-06	Supergraphics to Millennium Mills
W-AP-S2-07	Public space beacon at south west perimeter of Silvertown Quays Development
W-AP-S2-08	Stitch colour to DLR viaduct pier on North Woolwich Road at junction with Cunningham Avenue
W-AP-S2-09	High level element on Royal Crest Avenue at junction with Cunningham Avenue at Corinthian Square
W-AP-S2-10	Public space beacon at junction of Royal Wharf Walk and Bonnet Street
W-AP-S3-01	Stitch markers to be installed at regular intervals along route
W-AP-S3-02	High level element at junction of Capital Ring footpath and Stansfield Road
W-AP-S3-03	Stitch colour to DLR viaduct pier on Dockside Road towards Royal Albert DLR station
W-AP-S3-04	Area sign on north Connaught Bridge at Dockside Road
W-AP-S3-05	Area sign on south Connaught Bridge north of London City Airport
W-AP-S3-06	High level element at A112 and Connaught Road roundabout

Project Codes

W - Wayfinding Design Guide

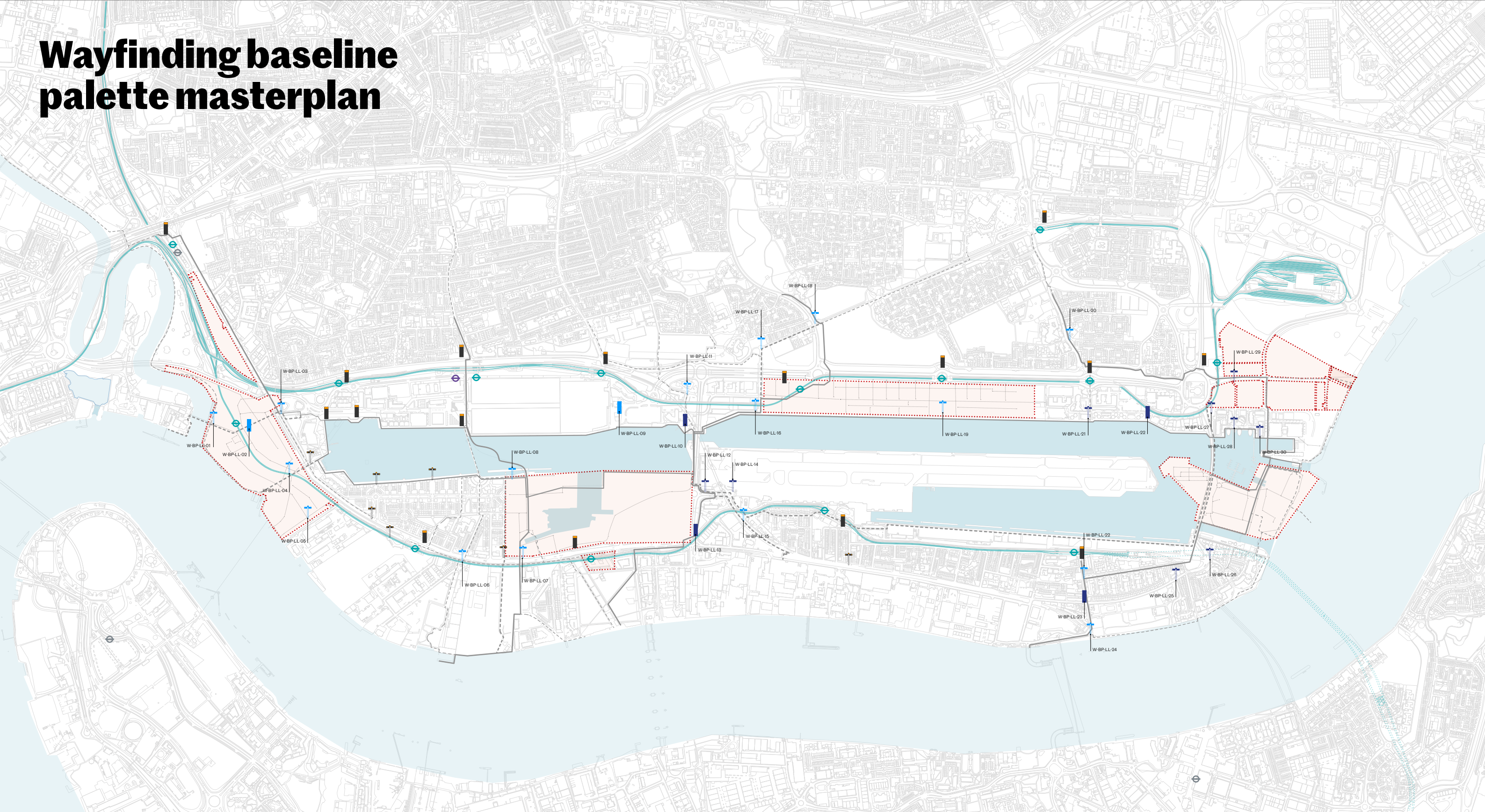
BP - Baseline Palette  
AP - Accent Palette  
ST - Special Treatments

W-AP-S3-07	Stitch colour to DLR viaduct pier at south east perimeter of Silvertown Quays Development
W-AP-S3-08	DLR viaduct pier on North Woolwich Road at Bramwell Way
W-AP-S4-01	Stitch markers to be installed at regular intervals along route
W-AP-S4-02	High level feature on Cyprus Place north of Cyprus DLR station
W-AP-S4-03	Public space beacon in forecourt of Gallions Reach DLR station
W-AP-S4-04	Supergraphics to Gallions Pump House
W-AP-S4-05	DLR viaduct at east perimeter of Gallions Reach Roundabout south of Atlantis Avenue
W-AP-S4-06	Public space beacon at junction of Armada Way and Gallions Road with Atlantis Ave
W-AP-S4-07	High level element at junction of Armada Way and Gallions Road with Atlantis Avenue
W-AP-S4-08	Area sign at turning of Hudson Way to Lockside Way
W-AP-S4-09	Area sign on dock edge at Woolwich Manor Way
W-AP-S4-10	High level feature at Pier Road junction with Woodman Street
W-AP-S4-11	Supergraphics to Woolwich Foot Tunnel North or boundary walls within the area
W-AP-S4-12	Public space beacon at Pier Road bus stop/lane east of Woolwich Foot Tunnel North
W-AP-S4-13	North Woolwich Pier and Ferry Terminal
W-AP-S5-01	Public space beacons on Siemens Brothers Way north of the Crystal
W-AP-S5-02	Dock Loop threshold mat on dock edge access from Siemens Way
W-AP-S5-03	Dock Loop active mat/insert to be installed at regular intervals along the dock edge
W-AP-S5-04	Dock Loop threshold mat on dock edge access from Fitzwilliam Mews
W-AP-S5-05	Maritime objects to be installed at regular intervals along route
W-AP-S5-06	Public space beacon in Royal Victoria Square
W-AP-S5-07	Dock Loop threshold mat at north entrance Royal Victoria Dock Footbridge
W-AP-S5-08	Dock Loop threshold mat at south entrance Royal Victoria Dock Footbridge
W-AP-S5-09	Dock Loop threshold mat at west of Millennium Mills at north west perimeter of Silvertown Quays Development
W-AP-S5-10	Public space beacons in Millennium Mills Square
W-AP-S5-11	Dock Loop threshold mat at south Connaught Bridge
W-AP-S5-12	Dock Loop threshold mat at north Connaught Bridge
W-AP-S5-13	Public space beacon on dock edge east of north Connaught Bridge
W-AP-S5-14	Public space beacon on dock edge at Royal Albert Quay south of junction with Royal Albert Dock

W-AP-S5-15	Public space beacon on dock edge south east of Beckton Park DLR station
W-AP-S5-16	Public space beacon on dock edge at Capital Ring footpath south east of Cyprus DLR station
W-AP-S5-17	Public space beacon on dock edge at Capital Ring footpath south west of Cyprus DLR station
W-ST-V-01	View from Silvertown Way viaduct east and west
W-ST-V-02	View from Crystal Gardens east over Royal Victoria Dock
W-ST-V-03	View from Thameside West over the River Thames south-west
W-ST-V-04	View from Royal Victoria Bridge east and west over the Docks' basin
W-ST-V-05	View from Connaught Bridge east and west over the Docks' basin
W-ST-V-06	View from Lyle Park over the River Thames south
W-ST-V-07	View from Royal Wharf Walk over River Thames south
W-ST-V-08	View from Thames Barrier Park over the River Thames and the Thames Barrier
W-ST-V-09	View from south-west corner of Beckton Park
W-ST-V-10	View from south-east corner of Beckton Park
W-ST-V-11	View from Steve Redgrave Bridge west towards London City Airport and the Docks' basin, and east towards Gallions Reach Marina
W-ST-V-12	View from Bascule Bridge west towards London City Airport and the Docks' basin
W-ST-V-13	View from Royal Victoria Gardens over the River Thames
W-ST-V-14	View from Gallions Point over the River Thames east
W-ST-V-15	View from Armada Green over the River Thames east
W-ST-S-01	Supergraphics to Silvertown Way Viaduct
W-ST-S-02	Supergraphics to Royal Victoria Dock cranes
W-ST-S-03	Supergraphics to West Silvertown chimney
W-ST-S-04	Supergraphics to Millenium Mills
W-ST-S-05	Supergraphics to Silo D
W-ST-S-06	Supergraphics to North Woolwich Road comercial buildings
W-ST-S-07	Supergraphics to West Silvertown chimney
W-ST-S-08	Supergraphics to DLR vaduct and piers
W-ST-S-09	Supergraphics to Connaught Bridge
W-ST-S-10	Supergraphics to Tate & Lyle sugar refinery
W-ST-S-11	Supergraphics to Crossrail wall on Albert Road
W-ST-S-12	Supergraphics to North Woolwich foot tunnel area
W-ST-S-13	Supergraphics to North Woolwich Ferry terminal
W-ST-S-14	Supergraphics to DLR wall on Newland Street
W-ST-S-15	Supergraphics to Steve Redgrave Bridge
W-ST-S-16	Supergraphics to Gallions Pumping Station
W-ST-S-17	Supergraphics to Royal Docks Pumping Station
W-ST-S-18	Supergraphics to Gallions Point slipway



# Wayfinding baseline palette masterplan



**MAP KEY**

Greater London Authority Project Sites

Stitch Route

**PALETTE KEY**

Legible London totem - existing  
Information points for navigation

Legible London totem - planned  
Information points for navigation

Legible London totem - proposed  
Information points for navigation

Legible London finger post - existing  
Directional signage for navigation

Legible London finger post - planned  
Directional signage for navigation

Legible London finger post - proposed  
Directional signage for navigation

**PROJECT CODES**

W-ST-LL-XX  
Legible London signage  
Refer to chapter 3, Palettes, for further guidance

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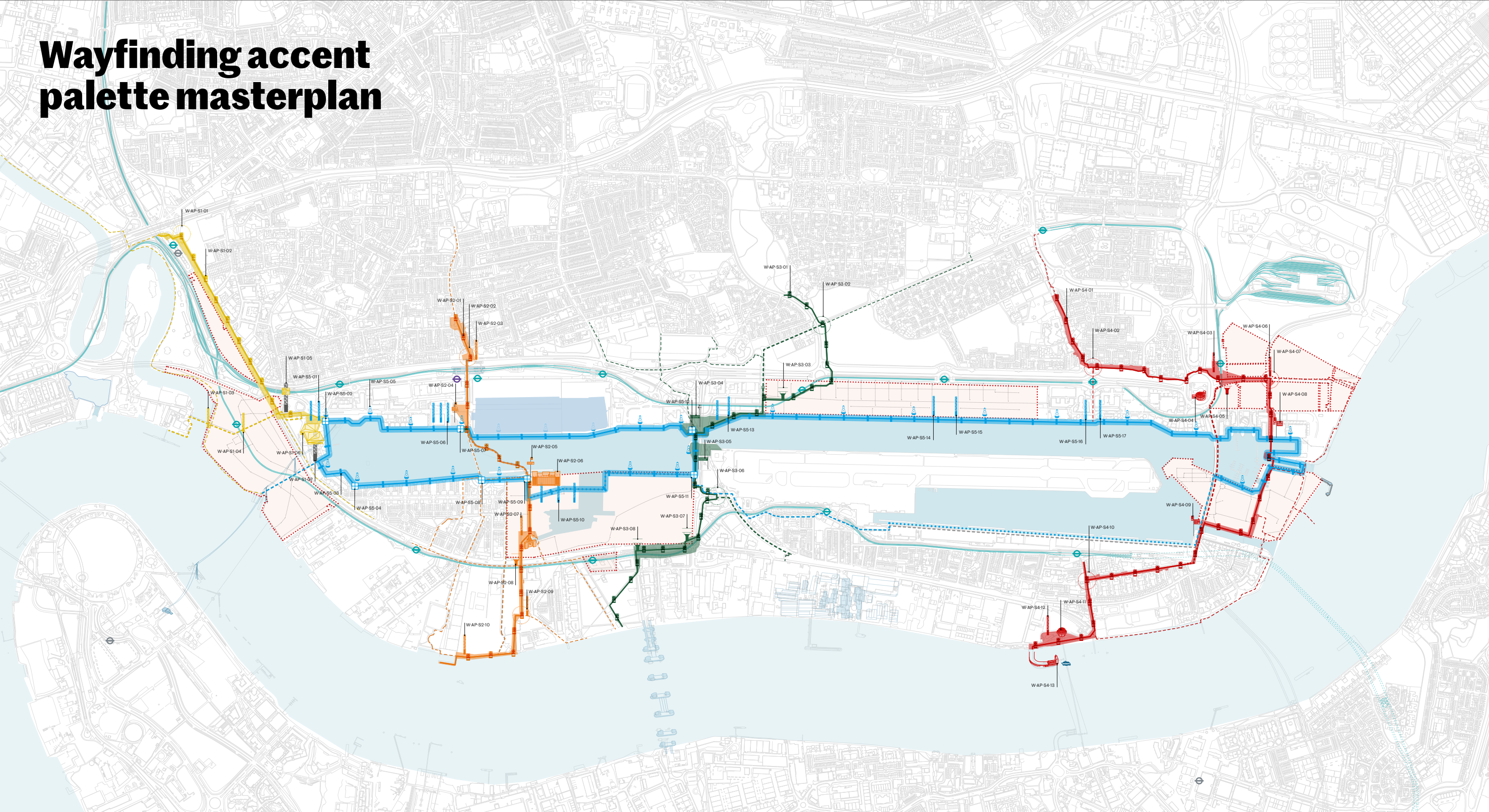
Unit 14  
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Project:		Royal Docks Public Realm Design Guides						
Drawing Title:		Wayfinding - Baseline Palette						
Scale:		1:7500@A1						
Status:		S2						
Drwg No:								
Job Code	Originator	Zone	Level	Type	Role	Number	Rev	
ROY- 5TH- XX- XX- DR- A-							P01	






# Wayfinding accent palette masterplan



MAP KEY




Greater London Authority Project Sites

PALETTE KEY

-  **Stitch 1: Canning Town to the Docks**  
*Stitch wayfinding characterised by yellow colour theme continued from the Leaway.*
-  **Stitch 2: Customs House to the Thames**  
*Stitch wayfinding utilises low-key signage, heritage structures and orange-corten colour.*
-  **Stitch 3: Connaught Crossing**  
*Stitch wayfinding incorporates playful super-graphics to infrastructure and structures that move in the wind.*

- Stitch 4: Beckton to North Woolwich**  
*Stitch characterised by 'light touch' red colour coding of existing street furniture; handrails, signage, bridges.*
- Stitch 5: Dock Loop**  
*Stitch wayfinding along the dock edge incorporates bespoke signage, maritime artefacts and long views.*


- Stitch 5: Dock Loop**  
Stitch wayfinding along the dock edge incorporating signage, maritime artefacts and long views.

-  Dock Loop threshold mat  
Installed at key access point to the Dock Loop
-  Dock Loop active mat / insert  
Installed at approximately 100m intervals identifying the Dock Loop and supporting active uses
-  Maritime objects  
Installed regularly along the Dock Loop

- Area sign  
Beacons installed at intersection of Dock Loop and the Stitches

- Public space beacons  
Lattice columns installed at public spaces along Stitches

- High level elements  
Additional large scale features, along Stitches

-  DLR viaduct piers  
Treatment to aid in wayfinding along Stitches

- Stitch markers  
Colour signs mounted on lighting columns along Stitch routes

PROJECT CODES

- |            |   |
|------------|---|
| W-AP-S1-XX | Stitch 1<br><i>Refer to chapter 3, Palettes, for further guidance</i> |
| W-AP-S2-XX | Stitch 2<br><i>Refer to chapter 3, Palettes, for further guidance</i> |
| W-AP-S3-XX | Stitch 3<br><i>Refer to chapter 3, Palettes, for further guidance</i> |
| W-AP-S4-XX | Stitch 4<br><i>Refer to chapter 3, Palettes, for further guidance</i> |
| W-AP-S5-XX | Stitch 5<br><i>Refer to chapter 3, Palettes, for further guidance</i> |

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Project:	Royal Docks Public Realm Design Guides
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Drawing Title: Wayfinding Baseline Palette

Scale: 1:7500@A1

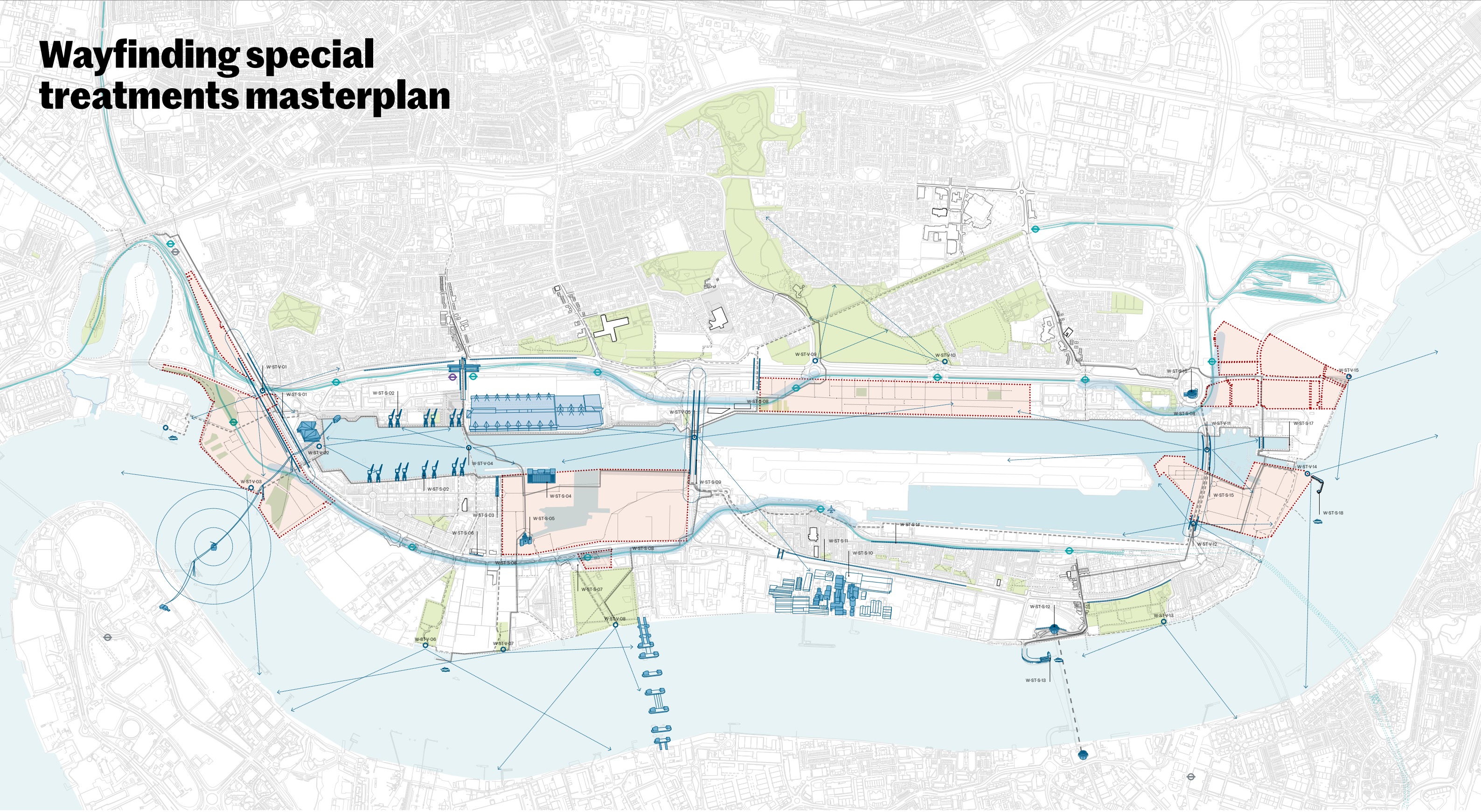
Status:	S2
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Drwg No:	
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ROY- 5TH- XX- XX- DR- A- 0062	P0
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# Wayfinding special treatments masterplan



**MAP KEY**

- Greater London Authority Project Sites
- Stitch Route

**PALETTE KEY**

- View points  
Long range vistas
- Landmarks  
Prominent structures in the area
- Bridges  
Key crossing points over the Docks

**PROJECT CODES**

- W-ST-V-XX Viewpoints  
Refer to chapter 3, Palettes, for further guidance
- W-ST-S-XX Streetscape  
Refer to chapter 3, Palettes, for further guidance

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Project: Royal Docks Public Realm Design Guides

Drawing Title: Wayfinding - Special Treatments

Scale: 1:7500@A1

Status: S2

Drwg No:							
Job Code	Originator	Zone	Level	Type	Role	Number	Rev
						ROY- 5TH- XX- XX- DR- A-	0063 P01



Royal Dock's designer's pack: Wayfinding design guide is a publication from the Royal Docks Team – a joint initiative from the Mayor of London and Mayor of Newham.

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